
TO: GENERAL COMMITTEE

SUBJECT: CITY COUNCIL 'BUMP-UP' REQUEST FOR SITE PLAN APPROVAL FOR THE SOUTH BARRIE SECONDARY SCHOOL (225 PRINCE WILLIAM WAY)

WARD: WARD 10

PREPARED BY AND KEY CONTACT: C. McLAREN, M.C.I.P., R.P.P., DEVELOPMENT PLANNER EXT.4719

SUBMITTED BY: S. NAYLOR, MES, M.C.I.P., R.P.P., DIRECTOR OF PLANNING *Steph Naylor*

GENERAL MANAGER APPROVAL: R. FORWARD, MBA, M.Sc., P. ENG. GENERAL MANAGER OF INFRASTRUCTURE & GROWTH MANAGEMENT *R. Forward*

CHIEF ADMINISTRATIVE OFFICER APPROVAL: C. LADD, CHIEF ADMINISTRATIVE OFFICER *CL*

RECOMMENDED MOTION

1. That the Site Plan Application as submitted by the Simcoe County District School Board, for lands located at 225 Prince William Way, be denied.
2. That the Site Plan Application by the Simcoe County District School Board, for lands located at 225 Prince William Way, only be approved subject to the plans being amended to reflect the relocation of the proposed Secondary School building toward the northern limits of the subject lands adjacent to Mapleview Drive East and the future extension of Prince William Way, as recommended by the Planning Services Department in Condition A (i) and (ii) of the Draft Site Plan Control Requirements for Processing attached as Appendix "A" to staff report PLN023-15.
3. That the Director of Planning Services or his designate be authorized to issue the Final Site Plan Control Requirements for Processing and a memorandum be provided outlining the Final Site Plan Control Requirements for Processing, once issued.
4. That the Owner/Applicant be exempt from the requirements of Section 5.7 of Site Alteration By-law 2014-100 in order to secure a Site Alteration Permit prior to the commencement of any works within the subject lands in accordance with By-law 2014-100.

PURPOSE & BACKGROUND

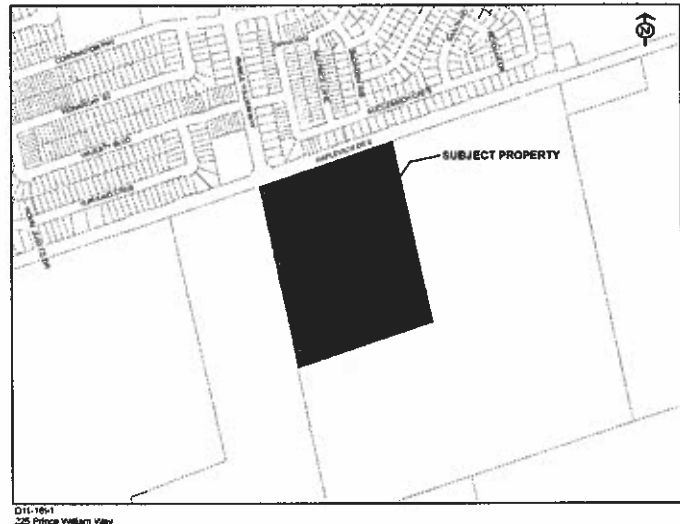
Report Overview

5. The purpose of this report is to recommend denial of the Site Plan Application submitted by the Simcoe County District School Board for the proposed 3-storey South Barrie Secondary School within the Hewitt's Secondary Plan Area. Staff are only recommending approval subject to the proposed school being relocated toward the northern limits of the subject lands.
6. The Site Plan Approval would normally be processed by way of delegated approval granted by Council to Staff. However in this case, the applicant has advised staff that they are not in agreement with certain conditions of the site plan approval. The Director of Planning Services and other City staff have met with the applicant and their consultants on several occasions and

were unable to reach a satisfactory resolution with respect to the placement of the buildings and athletic fields/running track on site. As such, Staff agreed that a “bump-up” to Council to recommend that the school be relocated on site would be the most appropriate mechanism for providing the applicant with a timely resolution on this matter.

Location

7. The subject lands are located on the south side of Maplevue Drive East, immediately east of future extension of Prince William Way within the Hewitt’s Secondary Plan Area. The property is legally described as Part of the North Half of Lot 18, Concession 11, Registered Plan SC1195803, formerly Geographic Township of Innisfil, now in the City of Barrie. The property is known municipally as 225 Prince William Way and has a total area of approximately 9.55ha (23.6 acres).



Existing Policy

8. The subject property is designated ‘Special Rural Area’ within the Town of Innisfil Official Plan, ‘Residential Area’ with provision for a Secondary School within the Hewitt’s Secondary Plan (OPA No.39) and is zoned ‘Agricultural General (AG)’ in accordance with the Town of Innisfil Zoning By-law 054-04.
9. Given the current Town of Innisfil ‘AG’ zoning over the lands, staff have identified a number of deficiencies on site including lot area, lot frontage, side and rear yard setbacks, parking, accessible parking, loading spaces and lot coverage. While the proposed use is considered to be a ‘Public Use’ in accordance with By-law 054-04 and would be permitted, Staff are of the opinion that the zoning standards for the AG zone were originally established to reflect that of a true agricultural use and therefore exceed the requirements for a typical institutional school use. As such, the applicant will be required to obtain Committee of Adjustment approval for all site deficiencies, failing which a Zoning By-law Amendment application and Council approval of same would be required.

Background

10. On March 2, 2015, a Pre-Consultation Site Plan Application was received from the Simcoe County District School Board for the proposed development. In accordance with normal practice, Staff from a number of departments met with the applicant and their consultants on March 26, 2015 to discuss the concept site plan submitted. Several standard design and technical comments were provided to the applicant at this meeting in addition to an alternative design consideration.
11. In this regard, in accordance with the policies of the City’s Official Plan and more specifically, the Hewitt’s Secondary Plan, the Planning Services Department identified that the proposed secondary school building should be relocated toward the northerly portion of the site and oriented toward the intersection of Maplevue Drive East and Prince William Way such that it would frame the abutting streets, particularly Maplevue Drive East; provide for a pedestrian friendly and accessible streetscape along the abutting arterial and major collector roadways; and act as a gateway to the Hewitt’s Secondary Plan Area.

12. The School Board, in response to Staff's comments regarding the location of the school, advised that the site had been designed with the proposed school located toward the southern portion of the lands such that it could take advantage of the existing topography and orientation of the site.
13. As requested by Planning Services through the pre-consultation process, a Draft Planning & Urban Design Brief prepared by the applicant's architect, ZAS Architects & Interiors, was received on May 22, 2015. This report served to provide further rationale for the placement and orientation of the school building on site. The report provided an overview of the expropriation history of the lands, site context, the proposed development concept; taking into account the existing topography and orientation of the lands that resulted in the proposed split-level building with northern exposure so as to maximize daylight and minimize southern heat gain into the school, and City of Barrie Planning Policy as it relates specifically to the City's Official Plan and the Hewitt's Secondary Plan.
14. Upon receipt of this report and further review of the preliminary development concept with a number of City departments, including the City's Engineering Department, Planning Staff were still of the opinion that the building should be relocated toward the northerly portion of the property so as to further achieve the design criteria established for development in the Hewitt's Secondary Plan.
15. Since the initial pre-consultation meeting in March, there have been a series of meetings with the School Board and their consultants to further discuss the location of the proposed school on site (see Appendix "B").

ANALYSIS

16. As noted above, the Simcoe County District School Board is proposing to develop the subject lands for the new South Barrie Secondary School with two associated athletic fields, a rubberized running track, parking and the potential for 15 future portables as illustrated on the Site Plan drawings attached as Appendix "C" to this report. The three-storey, split-level, 12,600m² structure, is intended to accommodate approximately 1,005 students.
17. The applicant is proposing that the school be constructed on the southern portion of the subject lands so as to take advantage of the existing topography and orientation of the site. In the opinion of the applicant's consultant, ZAS architects, the split-level school would act as a built-in retaining wall between the variable grade elevations of the site and the existing developable plateaus. In addition, the east-west orientation of the building is intended to maximize light exposure on the northern façade where a large expanse of glazing is proposed, while controlling heat gain on the southern façade. The north-south positioning of the athletic fields is intended to limit glare and shadows for users and would act as a 'grand entrance' to the Hewitt's Secondary Plan area.
18. The proposed development will be the first development to occur within those lands annexed from the Town of Innisfil in 2010 and extensive work has gone into planning for these lands over the past 5+ years. As such, Planning Staff are recommending that development of the subject lands be completed in accordance with the City's Official Plan and the design policies established through the Hewitt's Secondary Plan.
19. Mapleview Drive is identified as an arterial road and the future Prince William Way extension is identified as a major collector roadway within the Hewitt's Secondary Plan. Given the site's location with high traffic volumes and visual exposure, it is staff's opinion that the site is considered to be a gateway into the Hewitt's Secondary Plan area from the City's former southern limits, however it is not officially identified as a 'Gateway' within the Secondary Plan.
20. In light of the above, Planning staff feel that the proposed building must be relocated toward the northerly limits of the property, adjacent to Mapleview Drive East and future extension of Prince

William Way. A redesign of the site would ensure that the development is compatible with the existing built environment north of the subject lands and the anticipated future development of the Hewitt's Planning Area. In addition, the redesign would create an attractive and safe pedestrian realm, would support alternative modes of transportation (i.e. walking, cycling and transit) and would serve as a gateway to the Hewitt's Planning Area.

21. Staff have reviewed the subject site plan application in accordance with the design policies and objectives of the City's Official Plan, Hewitt's Secondary Plan and Site Alteration By-law 2014-100. A summary of the Planning review has been included as Appendix "D" to this report.

Site Alteration By-law 2014-100

22. Staff have been, and continue to work diligently with the applicant to resolve the balance of the technical site plan related matters, cost sharing obligations of the School Board with the Hewitt's Landowners Group, the timing of the approvals/construction of the future Prince William Way extension, and the timing/approvals associated with the proposed development. The applicant has expressed the urgency for commencing site alteration works as soon as possible in order to meet their anticipated opening timelines. In order to do so, the Engineering Department has advised that a Site Alteration Permit would be required for the proposed development.
23. As noted above, the subject lands are zoned Agricultural General (AG) in accordance with the Town of Innisfil Zoning By-law 054-04. However, section 5.7 of the City's Site Alteration By-law 2014-100 does not permit site alteration 'on any lands zoned Agricultural unless the owner provides a certificate, signed by a Qualified Person, confirming that the Site Alteration will result in maintaining or improving the overall fertility of the Agricultural Lands'. Given that the current applicable zoning of the property permits the proposed school on Agricultural zoned lands, the City's Engineering Department is recommending to Council that Section 5.7 of the Site Alteration By-law 2014-100 be waived for the proposed development to allow issuance of a Site Alteration Permit as referenced in the recommended motion. This would allow the applicant to proceed with site works in advance of the Site Plan being registered on title as per standard practice on other development sites throughout the City.

Summary

24. Based on the above, Planning Staff are recommending that the Site Plan Application, as submitted, is not in keeping with the City's Official Plan and the land use design policies established for the Hewitt's Secondary Plan Area. As such, staff are recommending to Council that the application be denied and the applicant be required to redesign their site such that the proposed secondary school building be relocated toward the northern limits of the subject lands, adjacent to Mapleview Drive East and Prince William Way, and that the proposed athletic fields/track be relocated to the southern limits of the site for the reasons identified above. Staff are satisfied that the balance of the technical site plan design matters will be adequately addressed through the standard Site Plan Approval process and will not require further Council approval.

Alternative Design

25. In support of the subject application, the applicant has submitted streetscape plans attached as Appendix "E" to this report for Council's consideration. Through consultation with the applicant, Staff are of the understanding that controlling vehicular access (including ATV's, snowmobiles, etc.) from accessing the proposed athletic fields, is a significant concern of the School Board. Should Council accept the location of the proposed secondary school in its current location, staff would require that the proposed streetscape plans be enhanced to address an appropriate pedestrian friendly interface between the subject lands and the public realm of Mapleview Drive East and the future Prince William Way extension as reflected in Condition A (iii) of the attached

Draft Site Plan Control Requirements for Processing. In this regard, Staff would require the provision of appropriate access control infrastructure such as a combination of landscaping, decorative bollards/pillars/armour stone and ornamental fencing, in order to avoid the enclosure of the property in its entirety with fencing. In addition, the provision of additional pedestrian oriented features such as street furniture, entry features, signage, gateway signage, public art, transit shelters, bicycle racks, etc., would be considered appropriate and would be required in some combination to enhance the street frontages of the subject property.

ENVIRONMENTAL MATTERS

26. While there are no environmental matters directly related to the recommended motion, staff note that the subject property is located within the Lake Simcoe Watershed and is subject to the policies of the Lake Simcoe Protection Plan (LSPP) and the City's Official Plan. While the Hewitt's Secondary Plan does not identify a Natural Heritage System within the subject lands, lands to the immediate west are identified as having a Natural Heritage System. In accordance with the LSPP and the City's Official Plan, when an application for development or site alteration is proposed within 120 metres of a key natural heritage or hydrologic feature, an Environment Impact Study (EIS) and/or Natural Heritage Evaluation (NHE) is required.
27. The subject lands are located within 120 metres of a 'wetland', therefore a scoped EIS and NHE has been completed to assess any potential impacts the proposed development may have on the adjacent unevaluated wetlands and potential significant wildlife habitat. This report concludes that there will be no negative impacts to identified key natural heritage/hydrologic features as a result of the proposed development.
28. The Lake Simcoe Region Conservation Authority is assisting the City in the final review and approval of all environmental related matters (natural heritage, stormwater management and hydrogeology – water balance) associated with the proposed development.

ALTERNATIVES

29. The following alternatives are available for consideration by General Committee:

Alternative #1 General Committee could agree with the Simcoe County District School Board that the southerly location of the proposed secondary school on the subject lands is appropriate (see Appendix "F") and that Draft Conditions A (i) and (ii) as they relate to the relocation of the proposed school and the athletic fields/running track, be deleted.

Planning Staff strongly recommend against this alternative as the current design is not in keeping with the design policies and objectives of the City's Official Plan and more specifically, the Hewitt's Secondary Plan. The proposed development will be the first development to occur within those lands annexed from the Town of Innisfil in 2010 and extensive work has gone into planning for these lands over the past 5+ years. This development will set the stage for all future development within the 'annexed lands', therefore there is an obligation of the City to ensure that the development is consistent with the Secondary Plan.

The Site Plan Application, as submitted, does not provide for a compact, pedestrian friendly and transit oriented built form, nor does it serve as a focal point for the Hewitt's Secondary Plan Area and it's neighbourhood as the building is not highly visible; it is not sited and massed toward the intersection; it does not provide high visual interest to pedestrians, or act as a 'sense of enclosure' to the street; it does not establish a strong street edge; and it does not establish an inviting public entrance through a generous public realm.

In staff's opinion, the only way to achieve these design initiatives, would be to relocate the building toward the northern limits of the site adjacent to Mapleview Drive East and the future extension of Prince William Way such that the main entrance(s) are located adjacent to the abutting roadways to encourage a pedestrian-oriented streetscape with social interaction, all while supporting active transportation and the use of public transit. A redesign of the site would ensure that the development is compatible with the existing built environment north of the subject lands and the anticipated future development of the Hewitt's Planning Area and would serve as a gateway to the Hewitt's Planning Area.

Alternative #2

General Committee could not exempt the applicant from the requirements of Section 5.7 of Site Alteration By-law 2014-100 and therefore require the applicant to register a Site Plan Agreement prior to any site alteration occurring on the subject lands.

This alternative is not recommended as the School Board would need to commence earth works as soon as possible in order to secure their funding and targeted opening date of the school.

FINANCIAL

30. The proposed Site Plan Application would permit the development of the new South Barrie Secondary School. The school, once opened, would be exempt from taxation and Development Charges are not applicable, however building permit fees are estimated to be in the order of \$237,712.00. A Parkland Levy is owing and is estimated to be approximately \$275,921.66 based on an assessed property value of approximately \$271,844.00 per acre.
31. The applicant will be required to enter into a cost sharing agreement with the Hewitt's Landowners Group for any future external infrastructures which they would receive benefit of, including the future extension of Prince William Way.
32. Through the Site Plan Application process, the applicant will be required to enter into a Development Agreement with the City that includes the payment of administrative fees and securities for site development.
33. The applicant would be responsible for all development costs associated with the proposed development, including the initial capital cost for the new external infrastructure required for the development, however following assumption, the infrastructure would be transferred to the City. Costs associated with asset ownership include ongoing maintenance and operational costs, lifecycle intervention expenses to ensure that assets reach their maximum potential useful lives as well as cost to ultimately replace (and possibly dispose) of the assets. The City would also incur additional operating costs associated with extending municipal services to the area such as fire protection, policing, boulevard landscaping maintenance and increased contributions to reserves to plan for the eventual replacement of the municipal assets.

LINKAGE TO 2014-2018 STRATEGIC PLAN

34. The recommendation(s) included in this Staff Report support the following goals identified in the 2014-2018 Strategic Plan:
 - Inclusive Community

35. A redesign of the subject site, as recommended by Planning Staff, will serve to promote and facilitate community connections and provide great public spaces that are highly visible, accessible, transit supportive and act as a "Gateway" into the Hewitt's Secondary Planning Area.

Attachments: Appendix "A" – Draft Site Plan Control Requirements for Processing
Appendix "B" – Meeting History
Appendix "C" – Reduced Site Plan Drawings
Appendix "D" – Planning Review Analysis
Appendix "E" – Preliminary Streetscape Plans
Appendix "F" – Letter from School Board
Appendix "G" – Infrastructure, Investment and Development Services
Committee Meeting Minutes - June 23, 2015

APPENDIX "A"

Draft Site Plan Control Requirements for Processing

File: D11-1691, 225 Prince William Way

SITE PLAN CONTROL REQUIREMENTS FOR PROCESSING

Section 41(13) b of *The Planning Act* allows Council to delegate by By-law Council's authority to approve site plans to an appointed officer of the Municipality.

Council By-law 99-312, as amended, has delegated Site Plan Approval authority to the Director of Planning Services, Manager of Development Control and Manager of Policy Planning. This authority permits the appointed officer(s) to recommend that the City Clerk prepare site plan agreements for execution and registration on title.

The appointed officer hereby grants Preliminary Approval to Site Plan Application File **D11-1691** located at **225 Prince William Way** on lands owned by **Simcoe County District School Board** upon registration of the development agreement for the above noted property to be prepared and completed in accordance with the following requirements:

This approval shall relate to the following plans as amended, if necessary:

	<u>Designer/Architect</u>	<u>Plan No.</u>	<u>Date/Rev.</u>
a) Site Plan	ZAS Architecture	A101	Aug 13, 2015/Rev #4
b) Building Elevations	ZAS Architecture	A401	Jun 19, 2015/Rev #3
c) Landscape Plan	MMM Group	LA-1, LA-2	May 1, 2015
d) Site Grading	Counterpoint Engineering	C-1	Jun 19, 2015/Rev #1
e) Site Servicing	Counterpoint Engineering	C-2	Jun 19, 2015/Rev #1
f) Erosion & Sediment Control Plan	Counterpoint Engineering	C-3	Jun 19, 2015/Rev #1
g) Notes & Details	Counterpoint Engineering	C-4	Jun 19, 2015/Rev #1
h) Electrical Site Plan/Details	MMM Group	E100, E101, E102	June 19, 2015

A conditional permit under Subsection 8(3) of the *Building Code Act* may be considered on its individual merits prior to the registration of a development agreement provided that in addition to the requirements under the Ontario Building Code the owner shall:

- agree in writing to satisfactorily address all conditions listed below;
- provide all required securities;
- provide all required administration fees, payment of costs associated with the preparation of a development agreement;
- provide a clearance letter or permit from the applicable conservation authority if required;
- comply with zoning by-law requirements;
- pay all applicable fees (i.e. building permit, cash in lieu of parkland, City of Barrie Act, development charges).

Prior to the appointed officer recommending that the City Clerk execute the Site Plan Agreement, the following requirements shall be satisfied and/or addressed:

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Revisions

- A. That the plans be amended to reflect the following:
- i) That the proposed building be relocated toward the northern limits of the site, adjacent to Mapleview Drive East and the future extension of Prince William Way;
 - ii) That the proposed athletic fields/running track be relocated toward the southern portion of the site and that the developed athletic field/running track be located adjacent to the future extension of Prince William Way;
 - iii) Provision of detailed streetscape plans/elevation drawings of the Mapleview Drive East and Prince William Way street frontages to address an appropriate pedestrian friendly interface between the subject lands and the public realm. In this regard, provision of appropriate access control infrastructure (i.e. combination of landscaping, decorative bollards/pillars/armour stone, ornamental fencing, etc.), street furniture, entry features, signage, gateway signage, bicycle racks, public art, etc.;
 - iv) Provision of a minimum 10m setback, inclusive of a 3m wide continuous landscape buffer, along the south and east property boundaries abutting all proposed portables;
 - v) Provision of elevation details of the proposed portables;
 - vi) That the fire department connection be relocated toward the south-west corner of the building;
 - vii) That the chain link fencing be extended southerly along the east boundary and continue along the south boundary to Prince William Way;
 - viii) Provision of a diagonal walkway into the southeast corner of the development for a linkage to a future subdivision walkway;
 - ix) Should Council approve the current location of the building, provision of a pedestrian connection from the bottom of the accessible ramps to the municipal sidewalk on Prince William Way, and deletion of the portion adjacent to the service driveway access;
 - x) Provision of municipal transit stop(s) to the satisfaction of the City's Facilities & Transit Department;
 - xi) Provision of a 0.3m reserve along all property boundaries, save and accept the approved entrance location(s);
 - xii) Provision of appropriate landscape strips along all property boundaries in accordance with the City's Urban Design Guidelines;
 - xiii) Provision of all proposed entrance feature signage including setbacks, elevation details and base landscaping of same;
 - xiv) Provision of bicycle racks;
 - xv) Provision of all rooftop mechanical units and the appropriate screening of same;
 - xvi) Provision of all transformers and the appropriate screening of same;
 - xvii) Provision of snow storage areas;

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- xviii) That the site plan drawings be amended as necessary to conform to the Engineering Department's technical requirements and current standards as identified in their comments dated August 4, 2015 (Parks Planning comments included); and
- xix) That the site plan drawings be amended as necessary to conform to the Lake Simcoe Region Conservation Authority's technical requirements and current standards as identified in their comments dated August 25, 2015.

Engineering

- B. The Owner shall enter into a cost sharing agreement with the Hewitt's Landowners Group for any future external infrastructures for which they will receive benefit.
- C. The Owner will be required to convey to the City of Barrie, a 26.0 metre wide parcel of land along the west limit of the property to support the future Prince William Way road allowance in accordance with the Multi Modal Active Transportation Master Plan (MMATMP) and as identified within the Hewitt's Creek Secondary Plan.
- D. That the Owner construct a temporary access/driveway from Maplevue Drive East that is located on their own lands (eastern limits preferred) to provide access to the new school in the most timely manner with relatively low throw-away costs and least disruption to the operation of the school prior to Prince William Way being constructed. This temporary access, could serve as a permanent "exit only" access (subject to Traffic Analysis and City approval) with provisions to support overflow parking for sporting events.
- E. That the Owner be required to convey to the City of Barrie, a 14 metre wide road allowance widening across the full frontage of the property and a 10 metre x 10 metre daylighting triangle at the intersection of Maplevue Drive East and Prince William Way in accordance with the Multi Modal Active Transportation Master Plan (MMATMP).
- F. That the Owner be required to provide a detailed Functional Servicing/Grading Report (signed and stamped by a licensed Professional Engineer) that demonstrates the existing municipal infrastructures (sanitary sewers, storm sewers, ditches, watermains, roadways and sidewalk systems) and utilities surrounding the subject property are adequate (size and condition) to support the proposed development and how those infrastructures will integrate into the future development of the Hewitt's Creek Secondary Plan. Any necessary improvements to the existing infrastructures or utilities would need to be identified within this report.
- G. That the Owner agree and understand that a temporary connection to the existing sanitary sewer on Prince William Way may be permitted if provided with an acceptable capacity analysis (considering existing and future scenarios). However, the Owner will be required to connect to the future Maplevue Drive sewer main when available. The existing sanitary sewer must be analysed from the proposed connection to the trunk sewer located at Hewitt's Creek.
- H. That the Owner be required to install a temporary bus pad and shelter (cash contribution for the shelter), along with a cash contribution towards the ultimate installation of a transit pad and shelter on the future Prince William Way roadway.
- I. The Owner will also be required to provide a detailed Stormwater Management Report (signed and stamped by a licensed Professional Engineer, complete with electronic files) that effectively addresses stormwater management and the impact this development may have on the receiving system with respect to quality and quantity control (post development flows shall be controlled to pre development flows for all storm events). This report shall confirm compliance with the Ministry of the Environment and Climate Change Stormwater Management Practices and Design Manual as it relates to enhanced level protection, and phosphorus loading and water balance as per the LSRCA Technical Guidelines for Stormwater Management Submissions, and any other City of Barrie applicable policies and guidelines.

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- J. The Owner will be required to provide a formal water service supply and demand analysis/report and model in support of the proposed water system ensuring adequate domestic and fire flow. The boundary conditions provided for this analysis/model must utilize results from a current fire flow test. The current report does not include the required model.
- K. The Owner will be required to provide a geotechnical report in support of this development. This report shall include a pavement structure recommendation, address ground water levels as they relate to servicing and building foundations and address soils as they relate to potential "Low Impact Development" initiatives.
- L. In accordance with By-law 2014-100, a Site Alteration Permit will be required for the subject development. In this regard, the Owner shall secure Council approval for exemption from Section 5.7 of the Site Alteration By-law, following which and prior to any site alteration within the subject property, the owner/applicant or his agents will apply for a Site Alteration Permit, as described within By-law 2014-100. Prior to the commencement of any works within the site, all requirements, obligations, and control measures, as described within By-law 2014-100 will be in place and undertaken to the satisfaction of the City of Barrie. Furthermore, it will be the owner/applicant's responsibility, through his professional consultant to maintain the said work for the duration of the subject property.
- M. The Owner will be responsible for obtaining the necessary approvals from any other applicable agencies with respect to their concerns.
- N. The Owner shall discharge any, and all, applicable City of Barrie Act or local improvement charges associated with the subject property.
- O. The Owner will be required to provide a formal water service supply/demand analysis in support of the proposed water system ensuring adequate domestic and fire flow, pressure and water quality. The boundary conditions provided for this analysis must utilize results from a current fire flow test.
- P. The owner will be required to provide the following drawings in accordance with the City of Barrie's Site Plan Application Manual and Urban Design Manual:
- i) Separate storm drainage catchment drawing (identify/accommodation any external drainage) and associated sewer design sheets (sewer design sheets to be included on the drawing; and
 - ii) Separate signage and pavement marking drawing in accordance with the Ontario Traffic Manuals. This drawing shall be detailed for use by the paint marking and signage contractor and shall include a legend, details, dimensions and material specifications.
- Q. The owner/applicant will be required to retain an experienced civil consulting engineer to provide the design, inspection, and certification of the installation of water and sanitary servicing for the proposed development, all to an appropriate connection/outlet. Detailed water servicing requirements are available through the Engineering Department.
- R. The owner/applicant will be required to retain an experienced civil consulting engineer to provide the design, inspection, and certification of the installation of the storm servicing works including parking lot construction and grading, all to the satisfaction of the Engineering Department.
- S. The owner/applicant will be required to retain a licensed experienced civil consulting engineer to provide a detailed Stormwater Management Report. The consultant will also be required to obtain, if necessary, MOE approvals for the implementation of any stormwater management works on-site, all to the satisfaction of the Engineering Department.
- T. The water distribution system within the limits of this site plan is privately owned and shall be maintained by the owner/applicant, and any hydrant installed on-site shall be deemed privately owned. All hydrants have to be maintained as per fire code/insurance requirements.

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- U. The owner/applicant will be responsible for obtaining a Right-of-Way Activity Permit prior to the commencement of work on the municipal right-of-way.
- V. That the drawings be revised as necessary to reflect the Digital Data Control Requirements. That the drawings be processed in digital format using UTM (Zone 17) NAD83 datum (76 adjustments).
- W. That an Electrical Site Plan be submitted and the owner agree and understand that all site lighting shall be arranged to deflect light away from adjoining properties and adjoining streets, and which will require full cut-off fixtures for exterior parking lot lighting and fully shielded fixtures for wall mounted exterior lighting. Shielded shall mean that 100% of the lumens emitted from the light fixture are projected below an imaginary horizontal plane passing through the highest point on the fixture from which light is emitted; all to the satisfaction of the Engineering Department.

InnPower

- X. That the owner complies with all requirements of InnPower (Innisfil Hydro) as related to electrical servicing for the development.

Parks

- Y. The owner will be required to retain a qualified Landscape Architect (and Arborist as applicable) to provide the design, inspection and certification of all landscape works, all to the satisfaction of the Parks Planning Section.
- Z. That the owner submit an Inventory/Assessment by a qualified consultant (or Arborist as applicable), of all existing vegetation and natural features on and adjacent to the site, with preservation recommendations and details to be approved and coordinated with the application for a Site Alteration Permit, and or Grading Plan submissions, all to the satisfaction of the Parks Planning Section.
- AA. That the landscape plans be amended, as required, to reflect recommended revisions concerning landscape areas, treatments, planting densities, screening/fencing, outdoor amenity spaces, site furnishings and pedestrian linkages, in accordance with the City of Barrie Urban Design Manual, all to the satisfaction of the Parks Planning Section.
- BB. That the owner/applicant provide a letter of clearance pertaining to the Endangered Species Act that demonstrates that the site is clear of any flora or fauna identified under the Act. The letter must be received prior to the commencement of any site works and as a condition of registration. In the event the site contains any endangered species, it is the responsibility of the owner/applicant to contact both the City of Barrie and the Ministry of Natural Resources and to take appropriate action.

Financial

- CC. That the owner pay the required cash deposits, securities and administration fees associated with site plan development for the following:
 - i) Letters of credit in the approved format and in accordance with Council Policy 07-G-016 for municipal works (such as roads and servicing) outside of the site plan boundary, equal to 100% of the value of works to the satisfaction of the Engineering Department;
 - ii) Proof of the owner's general comprehensive liability insurance policy in the amount of \$5,000,000 naming the City of Barrie as an additional insured;
 - iii) Administration fees for the review and inspection of site servicing and landscaping works equal to 5% of the estimated cost of site servicing (minimum \$1,000) and 5% for landscaping (minimum \$500) plus applicable taxes;

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- iv) Any water service charges arising out of, or attributable to the development of the site plan including tapping fee and water meter payment;
- v) Cash deposit for road cleanup associated with the site construction (\$2,000 refundable deposit made payable to the City of Barrie);
- vi) A retainer in the amount of \$4,000 payable to Legal Services Department, City of Barrie, for legal and administration fees associated with the preparation and registration of the site plan agreement (additional fees may be required);
- vii) Administration fees of \$1,850 associated with the City of Barrie Legal Services Department.

Planning and Building

- DD. That the owner obtain Committee of Adjustment approval for all site deficiencies, including but not limited to, lot area, lot frontage, side and rear yard setbacks, parking, accessible parking and loading spaces and lot coverage, failing which a Zoning By-law Amendment application and Council approval may be required.
- EE. That all required plans and reports (i.e. Archaeological Assessment, Traffic Impact Analysis/Parking Study, FSR/SWM, Environmental Impact Study, Tree Preservation/Assessment, etc.) be completed to the satisfaction of the City, the LSRCA, the Ministry of Tourism, Culture and Sport, and any other applicable agency as may be required.
- FF. That all sign locations be identified on the plans and details be provided and be in compliance with the City of Barrie Sign By-law 2005-093.
- GG. That the owner/agent ensures that all plans are consistent throughout.
- HH. That the owner agrees to the dedication of, or cash-in-lieu payment of parkland in accordance with the Planning Act, if applicable, in a manner satisfactory to the Parks Planning and Development Section and the Finance and Planning Services Departments.
- II. That the owner/applicant be responsible for obtaining the necessary approvals from any other applicable agency, if and as may be required.
- JJ. That the fire access route conforms to the Ontario Building Code and that the owner enters into a fire route agreement, if required.
- KK. That the owner agree and understand that all garbage and recycling containers are to be kept inside the building(s) in an appropriate garbage room or externally within an enclosure constructed of materials similar to that of the main building and screened from public view, otherwise the City shall act as the owner's agent and will have the containers removed at the owner's expense.
- LL. That all roof top mechanical devices be identified on the plans and shall be screened from public view by way of roof top location or by way of a parapet building extension to the satisfaction of the Planning Services Department.
- MM. That prior to the registration of the Site Plan Agreement; the applicant shall provide the Planning Services Department with the written confirmation that all conditions of PowerStream, the Lake Simcoe Region Conservation Authority, the Parks Planning & Development Section, Engineering, Fire Services, Building Services and the Planning Services Departments have been completed to their satisfaction.
- NN. That the final plans (4 complete plan sets and 3 engineering plan sets) be approved by the appointed officer and attached as schedules to the Site Plan Agreement. All final plans are to be stamped and signed by the associated professional consultant and a digital copy (high quality PDF without security settings) is to be provided prior to registration of the Site Plan Agreement.

File: D11-1691, 225 Prince William Way

- OO. The Site Plan Control Requirements shall remain in effect for a period of one year from the date referenced below following which a subsequent application may be required.
- PP. That if a building permit is not issued within two (2) years of this approval, this Certificate of Preliminary Approval shall become null and void.
- QQ. That the owner provide a letter or provide signature below, agreeing to the above Site Plan Control Requirements prior to any building permits, conditional or otherwise, being considered.

Stephen Naylor, M.C.I.P., R.P.P.
Director of Planning Services

Date

Owner acceptance and agreement with the SITE PLAN CONTROL REQUIREMENTS FOR PROCESSING

Please sign, date and return to the City of Barrie Planning Services Department.

Owner's Signature

Date

APPENDIX "B"

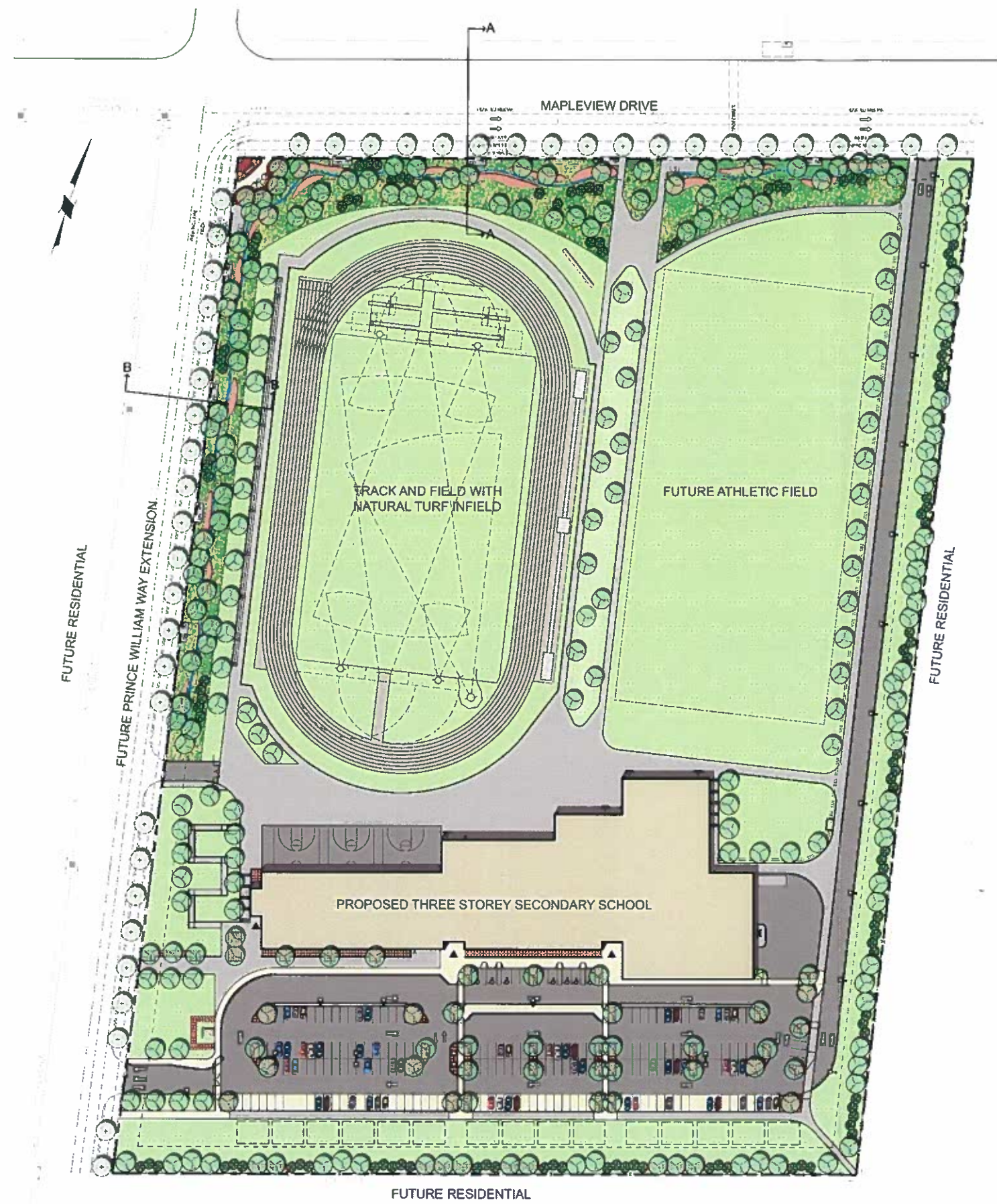
Meeting History

On June 10, 2015, Planning Staff met with the applicant to further discuss the concerns with the location of the proposed school on site but were again unable to reach a satisfactory resolution.

In an effort to continue to work with the applicant over the summer months and obtain some preliminary feedback from select Members of Council, the applicant requested to make a presentation to the Infrastructure, Investment and Development Services Committee (IIDSC) at their June 23, 2015 meeting. A presentation was made and the resolution of this meeting (15-G-167) was that a subsequent meeting be scheduled as soon as possible with the applicant, representatives of the Hewitt's Creek Landowners Group and City staff in order to develop a mutually acceptable location and design of the new South Barrie Secondary School (minutes attached as Appendix "G").

A follow up meeting was held on June 30, 2015 with all interested parties, however, the parties were unable to reach a satisfactory resolution with respect to the location of the building. It was however confirmed at this meeting by the Engineering consultants for both the School Board and the Hewitt's Landowners Group, that the existing topography of the subject lands did not preclude the development of the school toward the northern limits of the site as recommended by Planning Staff.

The technical site plan meeting was held on July 30, 2015 to address the formal Site Plan Application submission. While the placement of the building was still the primary issue of concern for Planning staff, a number of other technical site plan matters from various departments were discussed with the applicant at this meeting. The applicant and their consultants are continuing to work out the balance the technical site plan related matters and staff are confident that these issues will be adequately addressed and resolved through the typical site plan process.



PROJECT
SOUTH BARRIE
SECONDARY SCHOOL

DATE: 08/14/15
SCALE: 1:1000

CLIENT
Simcoe County
District School Board

1175 MONROE ST.
W. DELAWARE ONTARIO, L3L 1W8

LANDSCAPE ARCHITECT
ZIAS
LANDSCAPE ARCHITECTS
PLANNERS

2017 WILSON ST. #101
SCARBOROUGH, ONT. M1S 1T5
TEL: 416-291-1111
WWW.ZIAS.COM

CONTRACT NO. 15-001
DATE: 08/14/15

DATE: 08/14/15
SCALE: 1:1000

JOB PROJECT NO. 14-14006

1:1000 SCALE

LANDSCAPE PLAN

NO. 15-001-01-01-01
DATE: 08/14/15
SCALE: 1:1000

JOB PROJECT NO. 14-14006

1:1000 SCALE

LANDSCAPE PLAN

LA-1

APPENDIX "C" - REDUCED SITE PLAN DRAWINGS

PLN023-15
FILE: D11-1691
PAGE: 17

ELEVATION MATERIALS LEGEND

DM	CURTAIN WALL GLAZED SYSTEM
SM	WOODEN VOUCHER
PM	PAINTED WALL SYSTEM
MM	METAL SIDING
DM	INSULATED METAL DOOR
DM	ALUM GLAZED DOOR
DM	INSULATED SH DOOR
DM	ALUMINUM FRAMED GLAZED WINDOW
PM	PAINT FINISH METAL FLASHING
AM	PRE-FINISH ALUMINUM LINOLEUM
AM	AIR HANDLING UNIT. REFER TO MECHANICAL
TM	TRANSLUCENT TRANSLUCENT WALL SYSTEM

SOUTH BARRIE
SECONDARY SCHOOL

28 FRANK MILLERWAY
SOUTH BARRIE, ONTARIO

CLIENT



11 FRANKLIN ST
MIDLAND, ONTARIO, CAN. M5L 1A8

ARCHITECT



517 Phyllis St. Suite 101
Toronto, Ontario M5G 1K1
Tel: 416 979 8834
Fax: 416 979 8702

THIS DRAWING IS NOT TO BE BUILT

REFER TO APPROPRIATE SUBMITTALS: STRUCTURAL, MECHANICAL, ELECTRICAL, LANDSCAPE, ETC. CONSULT ALL APPLICABLE BUILDING REGULATIONS WITH THE LOCAL AUTHORITY.

CONTRACTOR SHALL VERIFY AND ACCEPT RESPONSIBILITY FOR ALL DIMENSIONS AND CONDITIONS ON SITE AND SHALL NOTIFY THE ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES FROM THE SUPPLIED INFORMATION.

THIS DRAWING IS AN INSTRUMENT OF SERVICE AS PROVIDED BY AND IS THE PROPERTY OF THE ARCHITECT.

THE CONTRACTOR SHALL VERIFY AND ACCEPT RESPONSIBILITY FOR ALL DIMENSIONS AND CONDITIONS ON SITE AND SHALL NOTIFY THE ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES FROM THE SUPPLIED INFORMATION.

KEY PLAN

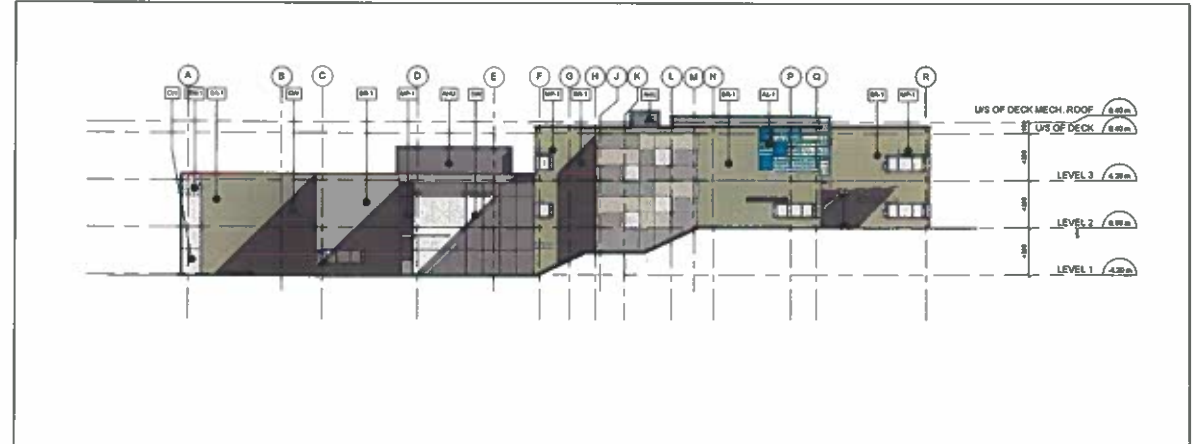
SCALE



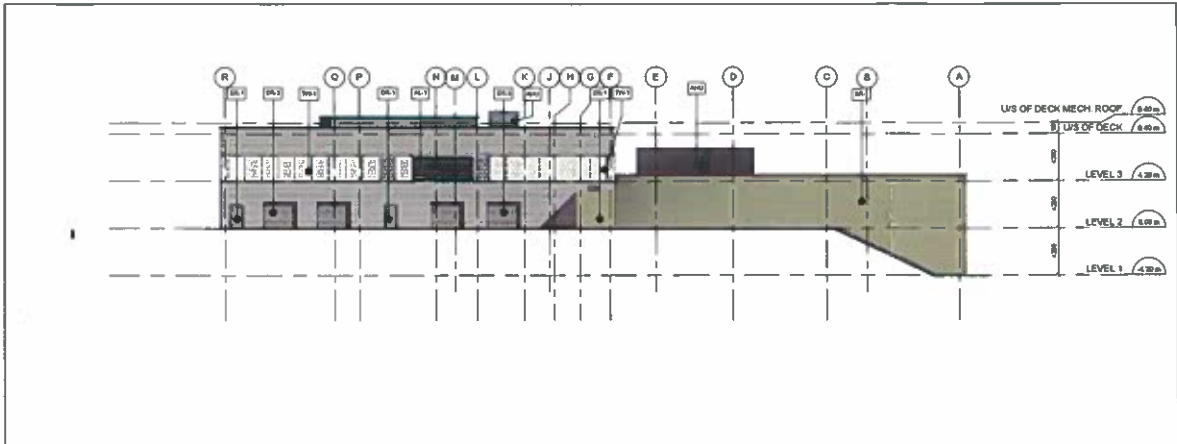
DATE: 2015 FEB 20

REVISIONS

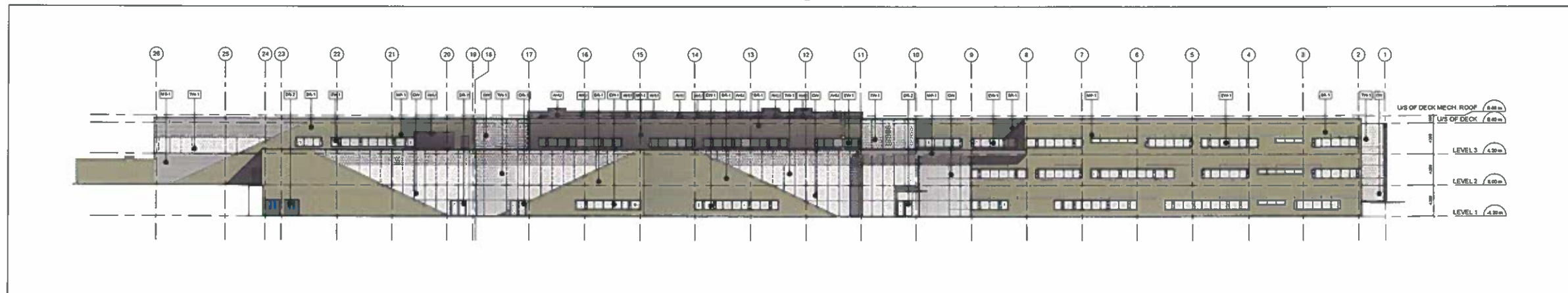
No.	Date	Description



4 WEST ELEVATION
1:200



3 EAST ELEVATION
1:200



2 NORTH ELEVATION
1:200



1 SOUTH ELEVATION
1:200

- 1 2015 JAN 16 ISSUED FOR SPA
- 2 2015 MAY 27 65% CHECKSET
- 3 2015 FEB 20 PRE-COMMUNICATION

No. Date Description

Project SOUTH BARRIE SECONDARY SCHOOL

65%CD ISSUED FOR SPA

Drawn by: JH Reviewed by: AG

Date: 09/16/15 Scale: As Shown

Sheet Title: 13-124

Building Elevations

SHEET NUMBER

A401

J&A ARCHITECTS + INTERIORS INC.

APPENDIX "D"

Planning Review Analysis

Official Plan

Section 3.0 of the City's Official Plan identifies a number of goals with respect to Growth Management. Section 3.1.1 (e) suggests that growth should be directed to take advantage of existing services and infrastructure where possible, and to minimize the cost of infrastructure extension. In this regard, relocating the school to the northern limits of the site, closer to the existing municipal sanitary, storm and watermain services, would be in keeping with the intent of this policy as the applicant would not need to extend services to the southern portion of the lands, thereby reducing the amount of throw away costs associated with temporary service connections to the southern limits of the site.

Section 6.5 of the City's Official Plan identifies general design guidelines to ensure the development and maintenance of a healthy, safe, convenient, efficient and aesthetically pleasing urban environment. While the applicant has demonstrated that a number of the Official Plan urban design guidelines are achievable with the development concept as submitted, staff feel that a redesign of the site would satisfy additional significant design policies.

In this regard, section 6.5.2.2 (a) the Plan suggests that buildings should be designed to complement and contribute to a desirable community character in terms of massing and conceptual design; building entrances should be well-defined and accessible to all pedestrians; and corner lots should emphasize the building, not the car as the dominant feature of the site. This section further provides that setbacks at corner locations should accommodate space for landscaping, pedestrian amenities and interesting architectural features.

While energy efficiency is encouraged through building design, section 6.5.2.2 (g)(iv) of the Official Plan suggests that energy efficiency is also promoted through the development of a compact urban form that encourages the use of transit, cycling and walking, a mix of housing and employment uses to shorten commuting trips and focusing major developments on transit routes. The relocation of the building toward the northern limits of the site would encourage energy efficiency through both building design and development of a compact urban form that supports active transportation and the use of municipal transit.

The proposed school is intended to accommodate in the order of 1,000+ students, is located at the intersection of two major roadways and would be the first development to occur within the Hewitt's Planning Area. As such, Planning Staff are of the opinion that the building should be relocated toward the northerly limits of the site adjacent to Mapleview Drive East so as to provide for a pedestrian friendly, transit supportive and accessible streetscape along the abutting arterial roadway. It is Staff's understanding that the majority of the student population for the proposed school is intended to be generated from the original "city limits" to the north of the subject lands. Therefore, positioning the building toward the northerly limits of the site would serve to provide the anticipated student population with convenient pedestrian and municipal transit access and would result in the building becoming the dominant feature of the site, as opposed to vacant athletic fields, thereby acting as a gateway to the Hewitt's Secondary Plan Area.

Hewitt's Secondary Plan (OPA No.39)

The Hewitt's Secondary Plan (OPA No. 39) serves to establish the general framework for the easterly portion of the 2010 annexation lands. Section 9.4.4.2 of the Secondary Plan identifies that all development shall be designed to be compact and have a pedestrian and transit oriented built form and major facilities, such as institutional uses, shall serve as focal points for the Hewitt's Secondary Plan Area and it's neighbourhood. This section further suggests that schools shall be visible, accessible and serve as 'meeting places' for residents.

Section 9.4.4.4 of the Secondary Plan identifies criteria for streetscape design and provides that arterial roads are high capacity streets which serve as major gateways to the City, while collector streets are thought to connect neighbourhoods to each other. Both arterial and collector streets are to have a high standard of design and must balance their transportation function and their ability to provide a socially vibrant public space.

The Secondary Plan further provides that buildings at arterial and collector roadways should be sited and massed toward the intersection; shall be designed to provide high visual interest to pedestrians, as well as a 'sense of enclosure' to the street; and establish a strong street edge. In addition, buildings shall act as focal points and establish an inviting public entrance through a generous public realm. In order to achieve the above, buildings and main entrance(s) should be located adjacent to the abutting arterial roadway (Mapleview Drive East) to encourage a pedestrian-oriented streetscape with social interaction, all while supporting active transportation and the use of public transit.

Section 9.4.4.12 of the Secondary Plan requires all public facilities, including school facilities, be designed to create compact neighbourhoods through the use of multi-storey buildings, joint use buildings/ parking areas/open spaces and the use of adjacent roads for visitor parking and other means to reduce land requirements. The Secondary Plan further provides that surface parking shall be minimized, screened from abutting streets and residential properties, located away from the street frontage and shall not generally be permitted in front of buildings.

The School Board has identified that the proposed development will serve as a regional attraction for track and field events as there is nothing of its kind in area and it is intended to be shared with a variety of user groups. As such, if the site were redesigned as recommended by Planning staff, this would provide the opportunity for a more centralized parking area between the building and the athletic fields/running track which would be screened from the abutting streets and adjacent residential properties and would accommodate the needs of the athletic events both during and after school hours.

As required by section 9.5.9.3(e) of the Secondary Plan, a key consideration in the design of the Secondary Schools within the Planning Area will be ensuring the efficient and effective use of land which encourages residents to walk, cycle and use transit to access facilities. To achieve this objective, consideration will be given to establishing maximum on-site parking requirements, use of lay-by facilities for drop-off/pick-up for school buses and general use; use of on-street parking; requirements for wider sidewalks and bike lanes on key access routes; and location on transit routes. Further, Secondary Schools are encouraged to be located where possible, away from residential uses and designed to minimize light and noise through measures such as separation distances, directional lighting, landscaping and fencing.

The School Board has demonstrated that some of the above policies may be achieved through the current design of the site. In this regard, the School Board is proposing a reduced surface parking standard on site which has been as supported by a Traffic Impact Analysis & Parking Study and approved by the City's Traffic Division; lay-by lanes for school buses and general use have been proposed; the proposed athletic fields/track are not intended to be lit; and all other parking lot and wall mounted light fixtures are required to be dark sky friendly, thereby minimizing light spillage onto neighboring properties and streets.

However in Planning staff's view, relocating the building to the northerly limits of the site as recommended, would serve to satisfy all of the Secondary School Land Use Policies identified above. The school would be located immediately adjacent to a major transportation route whereby municipal transit is provided, and further away from the future residential properties proposed along the southerly and easterly limits of the subject lands. Further, Mapleview Drive East would serve as an appropriate buffer between the existing residential properties to the north which back onto this arterial roadway.



STREETScape ELEVATION - MAPLEVIEW DRIVE EAST



STREETScape ELEVATION - FUTURE PRINCE WILLIAM WAY

APPENDIX "F"

Letter from School Board



Stephen Naylor
City of Barrie
70 Collier Street
Box 400
Barrie, ON
L4M 4T5

August 31, 2015

Re: **South Barrie Secondary School Site Plan Application (D11-1691)**

Dear Mr. Naylor,

Thank you for the opportunity to provide further input relating to the new South Barrie secondary school project that is currently under review by the City of Barrie. With the assistance and support of your staff, the Simcoe County District School Board (SCDSB) and its consultants have been working diligently over the preceding months to address issues related to the site plan application being considered for decision at the upcoming September 14, 2015 City of Barrie General Committee Meeting. We thank you for your commitment to this important project and making your staff available for the necessary meetings and information sharing.

We acknowledge that the site plan approval process in this instance is a complicated venture with many variables; SCDSB staff are confident that most challenges can be overcome within the existing planning framework. Currently at issue is the location of the school facility at the south of the property as shown in the submitted site plan application. While SCDSB and City staff continue the technical review process, the following is intended to summarize the context, site functionality, and redesign ramifications related to the location of the facility on the site. Our intent is to provide the rationale for the current design concept and identify the significant impacts of a complete redesign.

1. School Summary

The secondary school will be constructed on an approximately 20-acre parcel at the intersection of Mapleview Drive East and Prince William Way. This facility will provide the full spectrum of academic programming for secondary students with the added benefit of enhanced sports facilities currently nonexistent in the City of Barrie or County of Simcoe. These enhancements take the form of a dance studio/stage, exercise room, triple gym, rubberized track, natural turf field, and an additional practice field (a high performance artificial turf field is under consideration). The building is currently designed with views of the playing fields from the library, cafeteria, main entrance, and level 2 corridor, as well as the teaching areas facing north. The triple gym and student forum open directly onto a hard surfaced area with barrier-free student access to the playing fields. The design takes full advantage of the existing topography and positive environmental effects such as lighting and energy conservation.

2. Background

Board and City staff began consultation in the summer of 2011 as part of the Hewitt's and Salem secondary planning processes with regard to the SCDSB's future needs. It was through this process that the south Barrie secondary school location was determined. Rationale for this location and site configuration included availability of vacant land and existing servicing capacity, street access, geographic centrality to current and future student populations, the ability to provide sustainable attendance areas, maximizing the number of students within the walk zone thereby minimizing bussing requirements, opportunities for future facility expansion, the ability to provide enhanced athletic facilities (two athletic fields including a state-of-the-art track), and the consideration of potential partnership opportunities. In January of 2013, the Ministry of Education granted funding approval for this project totaling approximately \$27.5 million.

In the spring of 2013, the SCDSB and the City of Barrie initiated conversations relating to a potential partnership emphasizing a school with an enhanced athletic focus and, more specifically, the joint development of an artificial turf field on site. These conversations resulted in the development of a draft Memorandum of Understanding (MOU) between the City of Barrie and the SCDSB. It was partially on this basis that the concept plan for the school and double playing fields was initially developed; other considerations of the site design focused on site functionality and existing topography. After detailed and extensive conversations with the City of Barrie over the following year, and further solicitation of the private sector through a Request for Proposals, it was unfortunately determined that a financially viable

agreement could not be arranged at that time. Following this outcome, the concept plan was subsequently adjusted in the spring of 2014 to feature one athletic field with natural turf and rubberized track and a separate practice field. The SCDSB intends to continue to seek viable partnerships (including City of Barrie participation) for the development of an artificial turf field.

Through the balance of 2014 the SCDSB continued with the property acquisition, conversed with the City of Barrie regarding related issues, and began finalizing design drawings required for submission. In February 2015 a Site Plan Pre-Consultation Submission was provided to the City of Barrie. In March, a pre-consultation meeting was held, and in April SCDSB staff received written comments stating the building should be oriented towards the intersection of Prince William Way and Maplevue Drive East. The complete site plan application was submitted on June 19, 2015 and on June 23, 2015 SCDSB staff presented to the City of Barrie Infrastructure, Investment and Development Services Committee (IIDSC) to provide City representatives with the rationale for the design and to expedite the process. The IIDSC recommended a meeting between area developers, the SCDSB, and the City of Barrie which was subsequently held on June 30, 2015. The first technical review meeting was held on July 30, 2015 and SCDSB staff have since submitted an amended site plan and additional required information to address City of Barrie concerns without physically relocating the building. City of Barrie staff have been helpful with their suggestions regarding streetscape and site plan alternatives in this regard.

3. Urban Design Guidelines/Site Functionality

SCDSB staff and consultants have carefully considered the location of the building on the subject lands with respect to potential partnerships, the functionality of the school and site, interaction with surrounding land uses, natural drainage, existing topography, available servicing, and City of Barrie planning policy and design guidelines. With those considerations in mind, we believe the best location for the building is to the south as illustrated in the submitted site plan.

City of Barrie staff have commented - *"The building shall be relocated to the northerly limits of the site, thereby framing the abutting street(s). This would provide the opportunity for parking to be located between the building and the proposed sports fields, thereby providing convenient parking for the developed sports field/track."* This comment follows the pre-consultation comment - *"Consideration should be given to reorienting the building toward the intersection of Prince William Way and Maplevue Drive East, thereby framing the abutting streets. This would provide the opportunity for parking to be*

Page 4 of 7

located between the building and the sports field and reduce the cost associated with extending services to the southern portion of the site."

While City of Barrie staff have concerns regarding the location of the facility as it relates to urban form, SCDSB staff are equally concerned about the ultimate functionality of the school site when complete. School sites are entities that do not necessarily behave in the same way that residential, mixed use, or commercial uses do. It is the view of the SCDSB and their consultants that locating the building at the intersection of Mapleview Drive and Prince William Way would have detrimental impacts on overall school site functionality and create barriers to meaningful interaction with the surrounding community.

The Secondary Plan policies and Urban Design Guidelines recommend a high level of design for arterial streets, particularly in the mixed use corridor and nodes. It directs that buildings, structures and landscaping be designed to provide visual interest to pedestrians, as well as a sense of enclosure to the street. The school site is not located in a mixed use corridor or node, but in a residential area with existing reverse lot frontage on the north side of Mapleview Drive. Placing the school building at the street line of Mapleview Drive cannot create the same interactive urban environment that is desired in the mixed use corridors and nodes. Schools by their nature have increased security standards, not required of most other land uses, which are intended to control access during school hours in order to preserve student and staff safety. Locating the facility at the intersection, while perhaps aesthetically pleasing, would not enhance pedestrian and community interaction since the main pedestrian entrance would need to be associated with the parking, bussing, and supervised entrance. Any points of building access along Mapleview Drive and Prince William Way would need to be controlled and limited thereby effectively resulting in "dead walls" along these streets; this result would not align itself with the general intent of the Urban Design Guidelines and secondary plan policies. Locating the playing fields at the Mapleview Drive frontage can provide considerable visual interest, a view of the secondary school, and a pedestrian friendly active use interface as opposed to a 3-storey and approximately 550-foot wide secured structure.

Secondary Plan parking policies encourage buildings to be located at the street line and parking areas to be screened or placed to the side or rear of the building. It is believed that the impetus behind this policy is to avoid an expanse of parking abutting the sidewalk as evidenced in many existing commercial areas. City policy in general has shifted the focus away from the car and encourages active transportation and transit use. SCDSB staff and consultants believe the current layout is consistent with these secondary plan policies and achieves this by emphasizing the playing fields, providing bike racks, enhancing pedestrian

connectivity to the school, fields, municipal sidewalks and transit stops, and minimizing the presence of the parking area. City of Barrie staff also referenced convenience and accessibility as reasons parking should be located between the facility and the playing fields. Barrier free access to the fields from the school and parking area is shown on the submitted site plan. It has been the experience of the SCDSB that placing the parking area between the school and the playing fields should be avoided if at all possible as it requires students to frequently cross parking areas to access the fields creating potential safety issues; this can be particularly problematic during high volume events. Convenience for spectators is not in the best interest of students who will use the playing fields daily.

The City of Barrie Urban Design Manual provides an Urban Design Checklist to address physical environment and building siting, site circulation, site services, lighting, fencing, architectural design, signage, landscape design, waterfront, and transit. A number of the criteria related to physical environment and building siting are met by the proposed location of the school building at the southern limit of the site which are not met by the City's preferred location at the north end of the site. For example, the submitted location respects the existing topography and drainage while the alternate location would require extensive grading works. The split level building respects the existing scale and setbacks in the neighbourhood by presenting a smaller massing to the adjacent residences while a northern location would place a 3-storey structure at the street level and require extensive retaining walls and safety fencing at the southern portion of the property. The submitted location incorporates natural features, vegetation and topography by placing the playing fields next to the natural heritage system to the west and building the school into the existing topography, whereas the northern location ignores these features. The quality of views and influences of sun and wind were considered when siting the building to the south of the property. The amount of glazing on the north face is maximized which would not be possible if the building abutted the street. Views of the fields from the street and from the school are created from the submitted location at the south; placing the building at the street edge would not allow for views of the fields or provide perspective of the entire site itself. Site services such as the garbage enclosure, loading area, and transformer are located to the east of the building and are out of view from Mapleview Drive, placing the building closer to the street would bring these services into view from the arterial road. Active uses, in the form of the playing fields, are located at the street level in the submitted application, placing the building at the street edge instead would not achieve the desired effect of an urban streetscape with pedestrians moving in and out of buildings along the street since the school will be secured with controlled access.

The purpose of the preceding examples is not to critique the City of Barrie preferred building location. In any design there will be elements that do not explicitly adhere to the guidelines which therefore necessitates a degree of flexibility. As stated in the guidelines themselves, they "...are intended to be flexible. They are not intended to prescribe specific design solutions but rather to express the preferred design objectives of the City. The development process is viewed as a cooperative venture between the City and the development industry. A successful development will achieve the City's vision while meeting the individual needs of the proponent." It is the view of SCDSB staff and consultants that, with the assistance and guidance of City of Barrie staff, measures outside of building relocation can be taken to achieve the City's vision of urban design while at the same time meeting the school board's needs for a functional secondary school with high quality program opportunities and the residents' needs for an accessible focal point for community use and interaction.

4. Redesign Impacts

In addition to school functionality, the financial impacts of a full redesign would be substantial. Based on school functionality, servicing and storm water conditions, topography, and zoning bylaw and official plan requirements, SCDSB consultants have advised that simply relocating the existing design (with minor modifications) to an alternate location on the property would not be feasible. An entire redesign of the facility would be required. Redesign costs are conservatively estimated to be in excess of \$1 million with an estimated timeframe to complete of five (5) additional months at a minimum. This estimate is for consulting fees only and does not include any additional costs or benefits that may result from a new design. A full costs/benefits estimate is not possible at this time as this would rely on substantially completing a redesign and its associated studies.

It has been suggested that redesign could result in actual cost savings with regards to servicing which could be then applied to offset the redesign costs. SCDSB consultants acknowledge that it is possible some savings could result in the area of servicing while earth works costs could increase. It must be emphasized however, that any servicing or site preparation cost savings could not be applied to the actual design and construction costs of the facility and its amenities. Site acquisition, servicing, and site preparation costs as they relate to growth schools are funded through Education Development Charges (EDCs) and are not transferable to other aspects of construction projects. The EDC funding mechanism is "sweated" and thus can only be used for its intended purpose. Therefore, the estimated \$1 million plus in additional costs would need to come directly from the facility budget itself, meaning decreased square

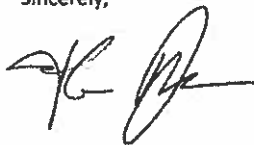
Page 7 of 7

footage, reduced and/or substandard specialized teaching spaces (gymnasium, athletic fields, technology, health and wellness, arts, science, etc.), and/or lower quality construction methods and materials. This would result in a diminished facility and, coupled with the associated time delay, would do a disservice to the students and residents of the community who will ultimately utilize this facility and its grounds for generations to come. These effects would be felt in some form for the life of this facility.

In closing, we are appreciative of the efforts put forth by City of Barrie staff to expedite the site plan approval process and to Council for consideration of this important project. We recognize that this school will be the first development to occur in the Hewitt's Secondary Plan area and is therefore subject to increased scrutiny. SCDSB staff are continuing to work with adjacent landowners, are keeping the school community informed, and will remain committed to amicably co-operating with City of Barrie staff in order to achieve a solution that meets the needs of all stakeholders. This project, when complete, will be a centrepiece of the existing and future community and the SCDSB respectfully requests that the rationale behind the site plan submission be carefully considered when weighing the delicate balance between urban form, functionality, responsible use of public resources, and the best interests of the students, residents, and community at large.

Thank you again for your consideration with regard to the new South Barrie secondary school and we look forward to continue working with City of Barrie Staff.

Sincerely,



John Dance

Superintendent of Facility Services
Simcoe County District School Board

cc: Director of Education, Simcoe County District School Board: Kathi Wallace
Board Chairperson, Simcoe County District School Board: Peter Beacock
Simcoe County District School Board: Trustees

APPENDIX "G"

Infrastructure, Investment and Development Services Committee - June 23, 2015 Meeting Minutes

URBAN DESIGN GUIDELINES - SOUTH BARRIE SECONDARY SCHOOL

John Dance, Superintendent of Facility Services, Simcoe County District School Board and Paul Stevens, ZAS Architects and Interiors Inc. provided a slide presentation concerning the proposed south Barrie secondary school and the Urban Design Guidelines.

Mr. Stevens discussed slides concerning the following topics:

- The location of the new secondary school site and surrounding uses;
- The proposed site plan and artist's renderings of the school and site;
- The School Board's rationale associated with the proposed site layout and the Board's view related to how it complies to the Official Plan and the Hewitt's Secondary Plan;
- The topography of the site and the impact of the topography on the design of the site including the location of the school and the two proposed athletic fields;
- The traffic considerations associated with the location of the parking lot, the 3 storey secondary school and the incidental drop-off locations;
- The elements of the City of Barrie's Official Plan, Urban Design Guidelines and Hewitt's Secondary Plan the he feels have met been within the design; and
- A secondary school built in Markham with a comparable design.

S. Naylor, Director of Planning Services and Carlissa McLaren, Development Planner, provided an overview of the surveys, consultation and concerns with respect to the proposed location and design of the south Barrie secondary school.

Members of the Committee and Council Members in attendance asked a number of questions related to the information provided and received responses from staff and the presenters.

Three individuals (Ray Duhamel, Keith McKinnon and Travis Doucette) addressed the Committee concerning the Urban Design Guidelines and the South Barrie Secondary School. The Individuals provided comments related to:

- The support for the development of a high school. The challenges associated with the school moving forward as the first development in the Hewitt's Secondary Plan;
- Possible precedent setting if the first development does not adhere to the Hewitt's Secondary Plan;
- The lack of inconsistency of the proposed site plan with the City's planning policies;
- The application being pre-mature without working through the drainage, grading, impacts on the road network, etc.;
- The consideration of cost sharing items;
- The impact on the sub-watershed and low impact options for consideration;
- The need for communication and information on the proposed phases, background studies and to be provided to the Hewitt's Creek Landowners Group and the Salem Land Owners Group;
- The concerns that the School Board are requesting special provisions and not abiding by the Urban Design Guidelines and the spirit of the Hewitt's Creek Secondary Plan;
- That the Site Plan be considered by City Council and not processed through delegated authority to staff;

Members of the Committee and Council Members in attendance asked several questions related to the information provided and received responses from staff and the presenters.

Attachments: Correspondence SCDSB.pdf

The Infrastructure, Investment and Development Services Committee met and recommends adoption of the following recommendation(s):

SECTION "B"

SOUTH BARRIE SECONDARY SCHOOL AND URBAN DESIGN GUIDELINES

That City staff facilitate a meeting as soon as possible involving representatives of the Simcoe County District School Board, the Hewitt's Creek Landowners Group and Planning staff in order to develop a mutually acceptable location and design for the new South Barrie secondary school in the Hewitt's Secondary Plan, and a representative of the Infrastructure, Investment and Development Services Committee be invited to attend the meeting. (15-G-114) (File: D00)

This matter was recommended to General Committee for consideration of adoption at its meeting to be held on 6/29/2015.