
TO: GENERAL COMMITTEE

SUBJECT: ZONING BY-LAW AMENDMENT APPLICATION 189, 191, 195, 197
DUCKWORTH STREET

WARD: 1

PREPARED BY AND KEY CONTACT: T. BUTLER, PLANNER, EXT. 5446

SUBMITTED BY: C. MCLAREN, RPP, MANAGER OF PLANNING

EXECUTIVE DIRECTOR APPROVAL: M. BANFIELD, RPP, EXECUTIVE DIRECTOR OF DEVELOPMENT SERVICES

CHIEF ADMINISTRATIVE OFFICER APPROVAL: M. PROWSE, CHIEF ADMINISTRATIVE OFFICER

RECOMMENDED MOTION

1. That the Zoning By-law Amendment Application submitted by Innovative Planning Solutions, on behalf of Cygnus Developments, to rezone lands known municipally as 189, 191, 195 and 197 Duckworth Street from “Residential Single Detached Dwelling Second Density” (R2) to “Residential Multiple Dwelling Second Density with Special Provisions” (RM2)(SP-XXX) be approved as attached to this Staff Report as Appendix “A”.
2. That the following site-specific provisions be referenced in the implementing Zoning By-law for subject lands:
 - a) Permit a minimum front yard setback of 4.0 metres, whereas a minimum front yard setback of 7.0 metres is required;
 - b) Require a minimum rear yard setback of 10.50 metres to the building, whereas a minimum rear yard setback of 7.0 metres would be permitted;
 - c) Require a minimum interior side yard setback of 24.0 metres, whereas a minimum side yard setback of 1.8 metres would be permitted;
 - d) That a minimum building setback to the daylighting triangle of 1.80 metres be permitted, whereas a minimum setback of 7.0 metres would be required;
 - e) That a minimum landscaped open space of 23% of lot area be permitted, whereas 35% of lot area would be required;
 - f) Permit a maximum Gross Floor Area (GFA) of 81%, whereas a maximum of 60% is permitted;
 - g) Permit a maximum building height of 12.80 metres, whereas a maximum height of 10.0 metres is permitted;
 - h) That a maximum density of 82 units per hectare be permitted, whereas a maximum density of 53 units per hectare is permitted;

- i) That an unconsolidated amenity area of 9.5 square metres per unit be permitted, whereas a consolidated amenity area of 12 square metres per unit is required;
 - j) That a consolidated amenity space of 126.0 square metres at grade be provided;
 - k) That a parking ratio of 1.2 spaces per dwelling unit be permitted, whereas a parking ratio of 1.5 spaces per dwelling unit is required;
 - l) That the location and storage of refuse within in-ground containers be permitted; whereas refuse is required to be located within the main building or an accessory building that is weather tight; containing a solid roof, walls, roll-up door and an exterior finish consistent with that of the main building.
3. That the written and oral submissions received relating to this application, have been, on balance, taken into consideration as part of the deliberations and final decision related to the approval of the application as amended, including matters raised in those submissions and identified within Staff Report DEV020-24.
 4. That pursuant to Section 34 (17) of the *Planning Act*, no further public notification is required prior to the passing of this By-law.

PURPOSE & BACKGROUND

Report Summary

5. The purpose of this report is to recommend approval of a Zoning By-law Amendment application submitted by Innovative Planning Solutions, on behalf of Cygnus Developments, for lands known municipally as 189, 191, 195 and 197 Duckworth Street. This report provides an analysis of the application based on conformity with Provincial and City Planning policies, as well as an overview of comments received through the technical review and public engagement processes.
6. With the conclusion of the technical review and public consultation process, which included a Neighbourhood Meeting on November 20, 2019 and Public Meeting on February 8, 2022, staff have determined that the proposal is consistent with the Provincial Policy Statement (2020) and conforms to the Growth Plan for the Greater Golden Horseshoe (2019), as amended, and the City of Barrie Official Plan (2010) which was in effect at the time the application was submitted. As such, staff are recommending approval of the rezoning application as outlined in the draft By-law attached to Staff Report DEV020-24 as Appendix "A."

Development Proposal

7. The application, if approved, would rezone the subject lands from "Residential Single Detached Dwelling Second Density" (R2) to "Residential Multiple Dwelling Second Density with Special Provisions" (RM2)(SP-XXX). The conceptual site plan, included for reference as Appendix "B" to Staff Report DEV020-24, proposes a development consisting of a three (3) storey stacked townhouse development containing a total of 24 dwelling units. The proposed elevations are attached as Appendix "C". The final design details for the site design will be confirmed at the time of a subsequent Site Plan Control application.

Site and Location

8. The subject lands are rectangular in shape and are comprised of four (4) existing lots located at the north-east corner of Mountbatten Road and Duckworth Street, south of Napier Street. The parcels are collectively 0.3 hectares (0.76 acres) in area, with frontage of 50.4 metres (165.35 feet) along Duckworth Street and flankage of 60.80 metres (199.48 feet) along Mountbatten Road. The subject lands are currently occupied with four (4) single detached residential dwellings, with one (1) dwelling unit on each property fronting onto Duckworth Street.

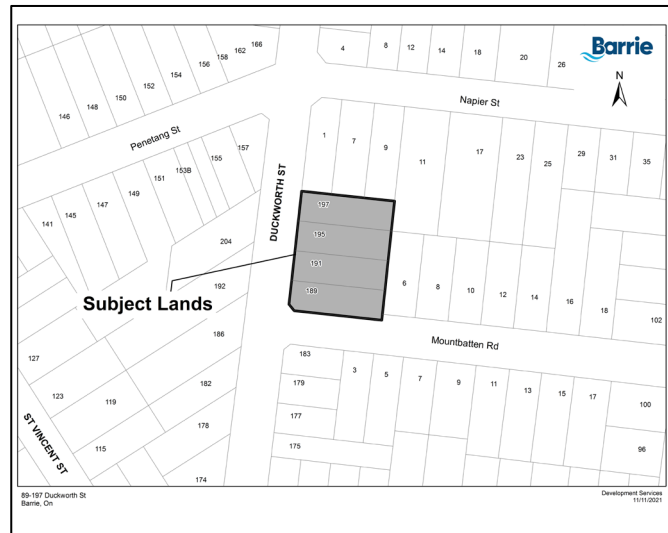
9. The surrounding land uses include the following:

North: Single-detached residential uses zoned R2.

East: Single-detached residential uses zoned R2.

South: Single-detached residential uses zoned R2 and institutional uses zoned "Institutional Educational" (I-E) - Codrington Elementary School

West: Single-detached residential uses zoned R2.



Existing Policy

10. The subject lands are designated 'Neighbourhood Area' on Map 2 – Land Use Designation of the City of Barrie Official Plan (2023) and are identified on Map 8 – Cultural City Features as being located within an Historic Neighbourhood – East End. The subject application was submitted to the City prior to the approval of the City's current Official Plan. In accordance with Policy 2.5.7 of the Official Plan (2023), applications deemed complete prior to the approval of the Official Plan, may continue towards final approval under the policy framework in place at the time the notice of a complete application was issued by the City. In this regard, the proposed Zoning By-law Amendment Application is subject to the policies of the former City of Barrie Official Plan (2010).

11. The former City of Barrie Official Plan (2010) designated the lands as 'Residential' as identified on Schedule 'A' – Land Use and are located along the Duckworth Street Primary Intensification Corridor in accordance with Schedule 'I' – Intensification Areas in the Official Plan. Lands located within the Primary Intensification Corridors are intended to develop at a target density of 50 units per hectare, collectively. Some properties may develop above this target density, while others may develop below the target or remain unchanged.

12. While the subject application is not being reviewed against the policies of the new Official Plan, Planning staff note that the proposed development includes a three (3) storey stacked townhouse building containing 24 residential condominium units at a density of 81 units per hectare, along an arterial road with existing and/or planned services and infrastructure, in accordance with the policy direction of the new Official Plan. The importance of providing an appropriate transition to lower density land uses has been considered through the building and site design. If approved, the proposed development would be subject to Site Plan Control where matters such as building design and materials, landscaping, stormwater management, and site servicing would be reviewed in greater detail.

13. The lands are currently zoned as “Residential Single Detached Dwelling Second Density” (R2) in the City’s Comprehensive Zoning By-law 2009-141, as amended.

Background Studies

14. In support of the application, the following plans, reports, and studies were submitted. Copies of the submission material are available online on the City’s Development Projects webpage under [Ward 1 – 189, 191, 195 Duckworth Street](#)
- a) Planning Justification Report (Innovative Planning Solutions, October 2023)
 - b) Functional Servicing Report (Pinestone Engineering Ltd., November 2023)
 - c) Traffic Brief and Parking Study (JD Engineering, January 2024)
 - d) Tree Inventory, Analysis, and Preservation Report (Landmark Environmental, October 2023)
 - e) Hydrogeological Assessment (Cambium Inc., November 2023)
 - f) Geotechnical Investigation (Cambium Environmental, October 2023)
 - g) Stormwater Management Report (Pinestone Engineering Ltd., November 2023)
 - h) Site Plan (Cari Cari Lee Architects, November 2023)
 - i) Shadow Analysis (Cari Cari Lee Architects, November 2023)

Public Consultation

15. A neighbourhood meeting was held on November 20, 2019. The meeting was attended by approximately 60 residents as well as the applicant, their consultant team, Ward 1 Councillor Clare Riepma, and Planning staff. The applicant’s team presented a previous concept at the Neighbourhood Meeting. Comments and concerns expressed by residents at the Neighbourhood Meeting included the following:

a) **Built Form Type**

Comments were made regarding a proposed apartment concept. Local residents described a desire to see low rise built form that would provide housing options for younger professionals and new families rather than an apartment building for student housing. Residents suggested that townhouses would be a more appropriate built form.

b) **Concerns regarding site access, increased traffic and on-site parking**

Concerns were raised regarding the provision of on-site parking, particularly as it relates to visitor parking. Members of the public wanted assurances that sufficient parking would be provided on-site to prevent the spill-over of on-street parking in surrounding neighbourhoods. Transportation Planning staff note that additional accesses onto Duckworth street could not be supported and confirmed their preference for access off of the abutting local street, Mountbatten Road.

The concept plan has been revised to reflect a three-storey stacked townhouse development, and parking is now being proposed at a ratio of 1.2 spaces per dwelling unit. In accordance with the Mixed Use (NMU) zone provisions that would be applicable to lands located on an “Intensification Corridor” (per Official Plan 2010), a parking ratio of 1 space per dwelling unit would be permitted. Duckworth Street is identified on Map 4B of the Official Plan for Transit Priority Features.

c) **Loss of privacy on neighbouring residential properties to the east**

Concerns were expressed regarding the height (6-storeys) of the proposed apartment building and the overlook condition it would have on the rear yards of the abutting single-detached residential properties to the north and east. The concept plan has been revised to reflect a 3-storey stacked townhouse development fronting Duckworth Street with increased separation distances between

the existing residential properties to the north and east (10.52 metres and 24.80 metres, respectively). The building is also separated from the north rear lot line and east side lot line by a consolidated amenity space, parking area and a 3 metre landscape buffer strip which will be planted with trees and include a 2 metre tight board fence along the adjoining lot lines. Planning staff are satisfied with the proposed design and are of the opinion that these design elements will appropriately mitigate any perceived privacy concerns.

d) Compatibility and Heritage Character

Questions were raised regarding the compatibility of the proposal as it relates to the character of the community as an older established area. City policies promote intensification and the efficient use of land and resources. It is anticipated that development through intensification and re-development will occur within more established neighbourhoods to meet City and Provincial growth intensification and housing targets. All levels of policy encourage a variety of housing types and forms, to contribute to the achievement of complete communities. The specific architectural details of the proposal will be formalized through a subsequent Site Plan Control application; however, staff are satisfied that the proposed building elevations provided in Appendix "C" of this report, would complement the existing development in the area.

16. A statutory Public Meeting was held on February 8, 2022 to present the subject application to the Affordability Committee and the public. No members of the public attended the public meeting.

Concept Plan Revisions

17. A previous concept plan had been reviewed by Planning staff prior to the current proposal being submitted for consideration [see Appendix "D" – Previous Concept Plan (2022)]. The applicant has made changes to the original concept plan submitted to address staff's technical feedback, and to address comments received through the Public Consultation process. These changes include:
- a) Reducing the proposed number of dwelling units from 28 to 24 units and consolidating the dwelling units into a single stacked townhouse block oriented toward Duckworth Street;
 - b) Altering the orientation of the building to engage with the Duckworth Street frontage where previously two separate stacked townhouse buildings were oriented internally;
 - c) Increasing the minimum north rear yard setback to 10.52 metres to the existing single detached residential properties fronting Napier Street in order to increase the separation distance and provide additional outdoor amenity area; and
 - d) Increasing the east side yard setback from 15.0 metres to 24.0 metres between the proposed building and existing single-detached residential properties to the east fronting Mountbatten Road and Napier Street.

Department and Agency Comments

18. The subject application was circulated to staff in various departments and external agencies for review and comment. No objections and concerns were received from the following departments:
- a) Development Services - Parks Planning, Urban Design/Heritage Planning, Addressing, Approvals Branch, and Transportation Planning
 - b) Building Department
 - c) Business Performance and Environmental Sustainability – Risk Management Official, Environmental Sustainability and Environmental Compliance
 - d) Finance Department

- e) Infrastructure Services Department (Water Services Branch)
- f) Engineering Standards
- g) Alectra Utilities, Hydro One and Bell Canada
- h) Simcoe County District School Board
- i) The Lake Simcoe Region Conservation Authority (LSRCA)

If approved, any comments that were provided will be addressed through a subsequent Site Plan Control application.

POLICY ANALYSIS

19. The following provides a review of the application in accordance with applicable provincial and municipal policy documents.

Provincial Policy Statement (2020) (PPS)

20. Staff have reviewed the relevant policies and are of the opinion that the development is consistent with the applicable policies of the Provincial Policy Statement (2020) which direct growth to settlements and identified growth areas and promote efficient, transit supportive development. The subject lands are located within the built boundary of the City and the application proposes intensification that is transit supportive. The proposed development aligns with the policies of a healthy, liveable, and compact community and would be serviced by existing municipal infrastructure and services, thereby representing efficient and cost-effective development.

A Place to Grow: Growth Plan for the Greater Golden Horseshoe (2019), as amended

21. Staff have reviewed the relevant policies and are of the opinion that the development conforms to the Growth Plan for the Greater Golden Horseshoe. The Growth Plan is intended to provide direction for municipalities in areas related to intensification of existing built-up areas with a focus on strategic growth areas, including 'Intensification Corridors' and major transit station areas. The primary focus of the plan is on building complete communities that are well-designed to meet people's daily needs, offer transportation choices, accommodate people at all stages of life by providing an appropriate mix of jobs, local services, public service facilities, and a full range and mix of housing options to meet various incomes and household sizes.
22. The proposed amendment will permit the development of high-density residential development in the existing built boundary at a density of approximately 81 units per hectare. The development is proposed to utilize existing/planned municipal servicing infrastructure, public facilities, is directly located on a future active transportation link, and on a transit route.

Former City of Barrie Official Plan (2010)

23. Staff have reviewed the relevant policies in the former City of Barrie Official Plan (2010) which apply to the application and are of the opinion that the proposed development conforms to the policies that guide development on the subject lands.
24. There are several of policies in the Official Plan that generally support the proposed development. Sections 2.3 Assumptions, 3.1 Growth Management, 3.3 Housing, and 4.2 Residential, relate to the provision of increased densities, directing growth to take advantage of existing services and infrastructure and the provision of a range and mix of housing types at appropriate locations.
25. The subject lands are designated 'Residential', as identified on Schedule "A" – Land Use in the City's former Official Plan. The predominant use of the 'Residential' designation is intended to provide areas for all forms and tenure of housing. The City's Official Plan also encourages

intensification in residential areas through residential conversions, infill, and redevelopment to promote an increase in planned and/or built densities, and to achieve a desirable compact urban form.

26. The subject property is located within the Duckworth Street Primary Intensification Corridor as identified on Schedule "I" – Intensification Areas of the City's Official Plan (see Appendix E – Schedule I Official Plan 2010). Lands located within Primary Intensification Corridors are intended to develop at a target density of 50 units per hectare, collectively.
27. Section 2.3 (d) and (e) of the Official Plan identifies that there will be a growing need to provide higher residential densities than previous development within the City and Barrie and new housing stock will include a growing percentage of multiple family development at medium and high densities in order to provide a complete range of housing options within the City. Intensification represents an essential component of the City's growth management strategy to minimize the infrastructure requirements of new development and to utilize existing services including transit, schools and open space. Further, increased density represents an opportunity to develop complete communities, as intended by the Growth Plan. The proposed development is considered to be consistent with this policy in that it proposes a high density residential development with an alternative housing type from what currently exists in the area, utilizes existing infrastructure and services, and would support the use of public transit.
28. Sections 3.3.2.1 (a), (b) (c) and (g) of the Official Plan encourage the maintenance of reasonable housing costs by encouraging a varied selection of housing with regard to size, density and tenure. The provision of innovative housing and a wide range of housing opportunities are encouraged in order to meet identified housing needs where it is recognized to be in accordance with good land use planning principles. The Official Plan further encourages residential intensification in built-up areas in order to support the viability of neighbourhoods and provide opportunities for a variety of housing types. Residential intensification includes infill development, which refers to the development of vacant or under-used parcels within existing urban areas.
29. Staff are satisfied the proposal conforms to these policies of the Official Plan given that the proposed development provides for an alternative housing form with a variety of unit sizes (1, 2 & 3 bedroom units), would contribute to a compact urban form and the efficient use of land and resources, supports transit, optimizes the use of existing infrastructure and services within a built-up area of the City, and is intensifying an underutilized site located along a designated intensification corridor as identified in Schedule "I" of the Official Plan.
30. Policy 4.2.1(g) states that it is a goal of the City to plan for new development in the medium and high density categories which encourage mixed use and high quality urban design within the Intensification Areas while continuing to support the integrity of stable neighbourhoods. As demonstrated throughout this report, consideration has been given to this policy as the proposed building has been strategically sited along the Duckworth Street frontage with increased rear and side yard setbacks to provide a greater separation from existing residential development. The building would also include quality building materials and a variety of architectural treatments to enhance the overall appeal of the development. The development also includes landscape strips provided at the required width (3.0 metres wide) along the rear and side lot lines to accommodate tight board fencing and buffer plantings. Finally, the applicant has demonstrated that the proposed building would not result in unreasonable shadowing on adjacent lands (see Appendix "F" – Shadow Study).

Affordable Housing

31. Section 3.3.2.2 identifies the goal to achieve a minimum target of 10% of all new housing units to be affordable. Although, there are no formally recognized affordable units identified for this site,

this project may assist with overall affordability, as townhouse units are generally considered a more attainable form of housing for prospective homebuyers, thereby increasing affordable options in the area. Additionally, the proposed development will increase the supply of available units within the City, thereby reducing market pressures that drive up housing costs, and subsequently provide additional opportunities for moderate income households to obtain suitable housing.

Urban Design Guidelines

32. Section 6.5 of the Official Plan provides that buildings should be designed to enhance and contribute to a desirable community character in terms of massing and conceptual design, the design of a building’s roof should screen mechanical equipment from public view, large exposed blank walls should be avoided, entrances should be well-defined, pedestrian links shall be designed to promote safety of the user and be fully accessible between commercial and residential properties, parking areas shall be linked and confined to rear yards, minimum planting strips shall be provided, and energy efficiency in design shall be encouraged. The proposed site layout and architectural design of the building would compliment the surrounding existing residential land uses. As identified on the site plan (Appendix “B” – Proposed Site Plan) and submitted elevation drawing (Appendix “C” – Conceptual Building Elevations) submitted in support of the application, the proposed building is oriented toward and massed along the Duckworth Street frontage, thereby creating an active and aesthetically pleasing streetscape. The proposed building incorporates increased side and rear yard setbacks and to reduce the perceived impacts of building height and massing on adjacent residential properties. Additionally, the subject application proposes a building with well defined entrances and pedestrian walkways with direct and safe connections to the municipal sidewalk on Duckworth Street.

Comprehensive Zoning By-law 2009-141

33. The applicant is proposing to amend the zoning on the subject lands from “Residential Single Detached Dwelling Second Density” (R2) to “Residential Multiple Dwelling Second Density with Special Provisions” (RM2)(SP-XXX), pursuant to City of Barrie Zoning By-law 2009-141.
34. The table below illustrates the required zone provisions and the proposed site-specific provisions as it relates to the proposed development. The zone standards identified in brackets represent the actual standards proposed by the applicant; however, Planning staff have made minor adjustments to some of the standards to provide flexibility at the time of detailed design and construction.

Zoning Standard (RM2)	Required	Proposed
Front Yard (min.) Table 5.3	7.0m	4.0 (4.03) metres
Setback to Daylighting Triangle	7.0m (front yard) 1.8m (side yard)	1.8 (1.87) metres
Landscaped Open Space (min. % of Lot Area) Table 5.3	35%	23% (23.3%)
GFA (max. % of lot area) Table 5.3	60%	81%
Building Height (max.) Section 5.3.2 (a)	10.0m	12.8 (12.73) metres
Density (max.) Section 5.2.5.1 (c)	53 units per hectare	82 units per hectare
Outdoor Amenity Space (min.) Section 5.2.5.2 (b)	12.0 sq.m. per unit (288.0 sq.m. consolidated)	9.5 sq.m. per unit (231.0 sq.m. total unconsolidated)
Parking Standards Table 4.6	1.5 parking spaces per dwelling unit (36 total)	1.2 parking spaces per dwelling unit (29 total)

<p>Waste Management for Multi-Unit Residential Developments Section 5.3.5.5 (a)</p>	<p>The storage of garbage, recyclables, and organic waste shall be within the main building or an accessory building that is weather tight;</p>	<p>In-ground waste containers shall be permitted and are not required within the main building or an accessory structure</p>
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Minimum Front Yard Setback & Setback to a Daylighting Triangle

- 35. The applicant is proposing a minimum front yard setback of 4 metres and a setback to a daylighting triangle of 1.8 metres adjacent to Mountbatten Road, whereas a minimum front yard setback of 7.0 metres is required within the Residential Multiple Dwelling Second Density (RM2) zone. Staff note that this setback is considered technical in nature given the configuration of the subject lands. In this regard, the shortest lot line abutting a street is deemed to be front lot line, therefore Mountbatten Road is considered the front lot line as it relates to the subject property.
- 36. Planning staff are of the opinion that the requested front yard setback of 4 metres adjacent to Mountbatten Road is appropriate and generally meets the intent of the By-law. Given the orientation of the building toward Duckworth Street, the defined front yard would represent that of a typical exterior side yard whereby a minimum setback of 3 metres would be permitted. In staff's opinion, this request is appropriate as the proposed development has been designed with an emphasis on the Duckworth Street frontage. The placement and orientation of the building toward Duckworth Street is intended to enhance the public realm, ensure seamless connectivity between the private development and the municipal sidewalk, and create a pedestrian and transit-supportive environment along Duckworth Street, all while respecting the existing development in the area.

Minimum Landscaped Area as a Percentage of Lot Area

- 37. While the Zoning By-law requires a minimum landscaped area of 35% of lot area, the applicant is proposing a minimum landscape area of 23% of the lot area. The general intent of requiring a minimum percentage of landscaped area is to ensure there is adequate permeable area for drainage, snow storage, amenity and opportunities for vegetation to be planted. As it relates to infill and redevelopment, landscaping is generally provided in the form of smaller hard-scaped and green space areas at-grade, and/or through the provision of green roofs or rooftop terraces. Parking is proposed to be provided internal to the site at grade with an area included to accommodate emergency services movements. These site conditions impact the provision of the required landscaped open space.
- 38. Development Services staff in the Engineering Approvals and Parks Planning Branches have confirmed that the reduction in landscaped open space will not compromise the site's ability to infiltrate stormwater and the site can accommodate adequate planting, in accordance with the prescribed planting densities in the City's Urban Design Manual. As such, Planning staff are of the opinion that the requested site-specific provision is appropriate as it will not compromise the functionality of the site or adversely impact surrounding land uses. Staff are satisfied with the request as the proposal includes space within the 'front yard' area along Duckworth Street and includes a landscaped amenity area to the north while also providing landscaped buffers that meet the requirements of the zoning by-law adjacent to the existing low rise residential dwellings along the side (east) and rear (north) lot lines. The provided areas are in general conformity with landscape open space requirements.

Maximum Gross Floor Area as a Percentage of Lot Coverage

- 39. While the Zoning By-law restricts the maximum gross floor area (GFA) of a development to 60 percent, the applicant is proposing a maximum gross floor area of 81 percent. The purpose of

regulating GFA and lot coverage is to ensure that sites are not overdeveloped, and that new development conforms to the envisioned character and growth of the neighbourhood. In staff's opinion, the proposed development is appropriate for the site as the design includes the fundamental components required to maintain the long-term functionality and viability of a residential infill development. In this regard, the proposal can be adequately serviced by municipal infrastructure and includes a safe vehicular access and pedestrian circulation network, sufficient parking, outdoor amenity areas, landscaping, snow storage areas, and waste/recycling storage facilities and collection areas. Additionally, the site design respects adjacent residential land uses through the strategic placement of the building closer to Duckworth Street, increased side and rear yard setbacks, and the provision of landscape buffer strips and tight board fencing to mitigate physical impacts (i.e. building height) on neighbouring properties and preserve privacy.

40. The increase in gross floor area as a percentage of lot area can be attributed to the altered building envelope being provided after accounting for revisions to the project through public and staff consultation. In accordance with the Zoning By-law, staff note that a maximum of 35% lot coverage would be maintained and does not require an exception. Planning staff are of the opinion that the requested site-specific provision is appropriate for a residential infill development of this nature, as the design/layout of the proposed development respects the character of the surrounding neighbourhood and site functionality has been demonstrated by the applicant.

Maximum Building Height

41. The application is proposing a maximum building height of 12.8 metres, whereas a maximum building height of 10 metres is permitted in the RM2 zone for stacked townhouse units as proposed. This increase in height is required to accommodate alternate unit types, some of which are proposed below grade. For General Committee's reference, it is important to note that walk-up apartment buildings are permitted up to a maximum height of 4 storeys or 20 metres in the RM2 zone.
42. The purpose of regulating building height is to maintain compatibility between built forms and to mitigate impacts to privacy and shadowing on adjacent properties, particularly in residential zones. The design and layout of the site respects the surrounding context, as it relates to maintaining compatibility with and preserving privacy on existing residential land uses to the immediate north and east.
43. Planning staff are of the opinion that the requested site-specific provision for increased building height is appropriate given the design of the proposed development and the context of the site. The provided setbacks have been increased to greater than 10 meters at the rear of the property adjacent to existing low rise residential buildings and the side yard setback to homes located on Mountbatten Road have been increased to 24 metres to reduce the impact of the provided balconies associated with each unit. In addition, a shadow study has been provided that illustrates only minor shadowing impacts on adjacent properties, with no property receiving shadowing for more than 4 hours per day.

Maximum Density

44. The application proposes a maximum density of 82 units per net hectare on the subject lands for a total of 24 dwelling units, whereas a maximum density of 53 units per hectare is permitted in the RM2 zone. It is noted that the property is located within the Duckworth Street Primary Intensification Corridor as identified on Schedule "I" of the City's Official Plan. Lands located within the Primary Intensification Corridors are intended to develop at a target density of 50 units per hectare, collectively. Some properties may develop above this target density, while others may develop below the target or remain unchanged.

45. The purpose of regulating residential density is to ensure that a site is not overdeveloped. When evaluating the density provisions of a development proposal, particularly in the case of infill and intensification projects, it is important to consider the functionality of the site, as well as compatibility with and transitions to adjacent land uses. Additionally, the regulation of density ensures that demand for City's services and infrastructure does not exceed the forecasted capacity. In this regard, it is staff's opinion that the site functionality is not compromised by the proposed density. A Density analysis is included as Appendix "G". The Density Analysis identifies that the current density of the neighbourhood study area is approximately 15.52 units per hectare. Should the proposed development be approved, the neighbourhood study area's overall density would increase to 15.91 units per hectare. Based on the foregoing, Planning staff are of the opinion that the proposed density of 81 units per hectare on this single development site is appropriate and maintains the purpose of the By-law.

Minimum Amenity Area (Unconsolidated)

46. The subject application proposes to permit unconsolidated amenity areas of 9.5 square metres per unit, whereas a minimum of 12 square metres per unit is required to be provided in a consolidated form.
47. The provided conceptual plan proposes unconsolidated amenity space at a rate of 9.5 square metres per unit, through the provision of a private balcony for each townhouse unit and a consolidated amenity space (126.0 square metres) at the northerly limits of the site. If approved, the remaining communal spaces will be appropriately maintained by a future condominium board and a portion of required cash-in-lieu of parkland charges could be utilized to support recreational opportunities at local area parks and community centres. Given the proximity of the existing public parks and open spaces including St. Vincent Park and the North Shore Trail (within approximately 650.0 metres), this site-specific request was deemed to be appropriate based upon the unique characteristics of the site and its location. However, as reflected in the recommended motion, staff are recommending that a minimum consolidated amenity space of 126.0 square metres be provided at grade in accordance with the concept plan provided.

Reduced Parking Ratio

48. The application proposes a parking ratio of 1.2 spaces per residential unit, whereas 1.5 spaces per unit is required by the Comprehensive Zoning By-law in the RM2 zone. It is noted however that if the Mixed Use zone was applied, a parking ratio of 1 space per dwelling unit would be required.
49. Planning staff are of the opinion that the requested parking ratio of 1.2 spaces/unit is appropriate and generally meets the intent of the By-law. A Transportation Impact Study and Parking Justification Study was submitted to support a reduction in the parking space standard to the satisfaction of Transportation Planning staff. A reduced parking standard is considered an appropriate design consideration to encourage walkable communities, multi-modal transportation options and transit use, and is understood to support overall affordability. The subject lands are suitably located for ease of public transit use with local bus service running along Duckworth Street in addition to local convenience commercial at the intersection of Penetang Street and St. Vincent Street and institutional uses located within walking distance (i.e. Bethel Community Church and Codrington Elementary School).

Waste Management for Multi-Unit Residential Developments

50. The application is proposing to accommodate the storage of refuse within in-ground containers located behind the building adjacent to the parking lot along the east lot line. The zoning by-law requires that when more than 6 dwelling units are proposed, that the storage of refuse materials

must occur either within the building or within an accessory structure screened from view of the street.

51. While this is a site plan control related matter, the owner has indicated that refuse removal from the site will be by way of private waste collection. As such, the inclusion of in-ground refuse containers is satisfactory to staff as the in-ground containers would be placed in a location that is screened from view of the street while providing adequate access for both residents and waste contractors.

Minimum Rear (North) and Interior Side (East) Yard Setbacks

52. While no other site specific zoning provisions would be required to implement the proposed development concept in accordance with the proposed RM2(SP-XXX) zoning over the subject lands, staff are recommending that the approval of the subject application be tied to the proposed concept plan (see Appendix "B"). In this regard, staff are recommending that the following additional and more restrictive site-specific zoning provisions be incorporated into the implementing zoning by-law for the subject lands:
- a) That a minimum rear (north) yard setback of 10.5 metres be provided, whereby 7 metres would be permitted;
 - b) That a minimum interior side (east) yard setback of 24 metres be provided, whereby 1.8 metres would be permitted; and,
 - c) That a minimum consolidated amenity space of 126.0 square metres be provided at grade, whereby 288.0 square metres of consolidated amenity space would be required.
53. In staff's opinion, the proposed site layout is functional and the site-specific zoning provisions proposed would result an appropriate form of residential infill development within an existing established area of the City on a site that is currently underutilized. Based on the foregoing, Planning staff are of the opinion that the requested site-specific provisions represent good planning and would facilitate the development that is consistent with the City's policies, guidelines and goals.

Development Generally in Accordance with the Conceptual Site Plan

54. Planning staff are recommending that the subject lands be developed generally in accordance with the Conceptual Site Plan submitted in support of the application, as it relates to building height, placement and setbacks, as well as the location and configuration of landscape strips, drive aisle and parking area (see Appendix "B"). The Conceptual Site plan would be included as a schedule to the implementing zoning by-law. This site-specific provision will provide clarity as it relates to the zoning provisions for the site and ensure that the development, as proposed, is realized at the time of construction.

Site Plan Control

55. Subject to Council approval of the proposed application, the property would be subject to Site Plan Control as per Section 41 of the *Planning Act* and in accordance with By-law 99-312. Site Plan Control addresses the detailed design elements of the lands, access, servicing, stormwater management, landscaping, lighting, zoning compliance, building orientation/placement/massing and parking. The Site Plan process will ensure the development complies with all municipal standards and provides an appropriate interface with adjacent properties and streets.
56. Should the subject application be approved, staff are satisfied that the detailed design elements would be adequately addressed through a subsequent site plan application.

ENVIRONMENTAL AND CLIMATE CHANGE IMPACT MATTERS

57. There are no environmental and/or climate change impact matters to be considered for this application.

ALTERNATIVES

58. The following alternatives are available for consideration by General Committee:

Alternative #1 General Committee could refuse the proposed Zoning By-law Amendment application and maintain the existing “Residential Single Detached Dwelling Second Density” (R2) zoning over the subject property.

This alternative is not recommended as the proposed development is consistent with and conforms to all Provincial and municipal policies as noted throughout the analysis section of this report and is considered an appropriate use of the subject lands.

Alternative #2 General Committee could approve the proposed Zoning By-law Amendment application without the requested Special Provisions (SP).

This alternative is not recommended as the applicant has submitted a detailed concept plan which is generally consistent with the current City standards with respect to building orientation/placement/massing, setbacks, access, parking, landscaped areas, etc. The site-specific provisions have been recommended to provide residents with some level of assurance that the concept plan submitted will be realized at the time of site development and takes into account changes to the proposal based on community feedback.

FINANCIAL

59. The proposed development will be subject to development charges, cash in lieu of parkland, an education levy as well as building permit fees as governed by legislation, regulations, and City by-laws. All costs associated with the approval and development of the site would be the applicant’s responsibility. The applicant would be responsible for all capital costs for any new infrastructure required within the development limits and any of the costs associated with upsizing municipal water and sewer mains already installed, and traffic turning lanes or signals if required.

LINKAGE TO 2022-2026 COUNCIL STRATEGIC PLAN

60. The recommendation(s) included in this Staff Report support the following goals identified in the 2022-2026 Strategic Plan:

- Affordable Place to Live
- Community Safety
- Thriving Communities

61. In accordance with Council’s goals, the proposed development would provide for a compact form of development that will utilize existing services and infrastructure, provide additional dwelling units and add the potential for more affordable and attainable housing options, and support the local and regional transit network.

Attachments: Appendix "A" – Draft Zoning By-law Amendment
Appendix "B" – Conceptual Site Plan
Appendix "C" – Proposed Concept Building Elevations
Appendix "D" – Previous Concept Plan (2022)
Appendix "E" – Schedule "I" - Intensification Areas (Official Plan 2010)
Appendix "F" – Four Season Shadow Study
Appendix "G" – Residential Density Analysis

APPENDIX "A"

Draft Zoning By-law Amendment



Bill No. XXX

BY-LAW NUMBER 202X-XX

A By-law of The Corporation of the City of Barrie to amend By-law 2009-141, a land use control By-law to regulate the use of land, and the erection, use, bulk, height, location and spacing of buildings and structures in the City of Barrie.

WHEREAS the Council of The Corporation of the City of Barrie deems it expedient to amend By-law 2009-141 to rezone lands legally described as: Lot 4, Lot 5, Lot 6, and Part Lot 7 on Plan 1104 in the City of Barrie, County of Simcoe, known municipally as 189, 191, 195 and 197 Duckworth Street, shown on Schedule "A" attached to this By-law from 'Residential Single Detached Dwelling Second Density' (R2) to 'Residential Multiple Dwelling Second Density - Special Provision No. XXX (RM2)(SP-XXX).

AND WHEREAS the Council of The Corporation of the City of Barrie adopted Motion 24-G-XXX.

NOW THEREFORE the Council of The Corporation of the City of Barrie enacts the following:

1. **THAT** the zoning map be amended to change the zoning from 'Residential Single Detached Dwelling Second Density' (R2) to 'Residential Multiple Dwelling Second Density - Special Provision No. XXX (RM2)(SP-XXX) in Comprehensive Zoning By-law 2009-141, in accordance with Schedule "A" attached to this By-law being a portion of the zoning map.
2. **THAT** notwithstanding the provisions set out in Table 4.6 – Parking of Comprehensive Zoning By-law 2009-141, the minimum required parking ratio shall be 1.2 parking space per unit in the Residential Multiple Dwelling Second Density - Special Provision No. XXX (RM2)(SP-XXX) zone.
3. **THAT** notwithstanding the provisions set out in Section 5.3.1 of By-law 2009-141, a minimum landscaped open space area of 23% of lot area is permitted in the 'Residential Multiple Dwelling Second Density – Special Provision No. XXX' (RM2)(SP-XXX) zone.
4. **THAT** notwithstanding the provisions set out in Section 5.3.1 of By-law 2009-141, a minimum front yard setback of 4.0 metres to the front lot line and 1.8 metres to a daylighting triangle is permitted in the 'Residential Multiple Dwelling Second Density – Special Provision No. XXX' (RM2)(SP-XXX) zone.

5. **THAT** notwithstanding the provisions set out in Section 5.3.1 of By-law 2009-141, a minimum interior side (east) yard setback of 24.0 metres is required in the 'Residential Multiple Dwelling Second Density – Special Provision No. XXX' (RM2)(SP-XXX) zone.
6. **THAT** notwithstanding the provisions set out in Section 5.3.1 of By-law 2009-141, a minimum rear (north) yard setback of 10.0 metres is required in the 'Residential Multiple Dwelling Second Density – Special Provision No. XXX' (RM2)(SP-XXX) zone.
7. **THAT** notwithstanding the provisions set out in Section 5.3.1 of By-law 2009-141, a maximum gross floor area as a percentage of lot area of 81% is permitted in the 'Residential Multiple Dwelling Second Density – Special Provision No. XXX' (RM2)(SP-XXX) zone.
8. **THAT** notwithstanding the provisions set out in Section 5.3.1 of By-law 2009-141, a maximum building height of 12.8 metres is permitted in the 'Residential Multiple Dwelling Second Density – Special Provision No. XXX' (RM2)(SP-XXX) zone.
9. **THAT** notwithstanding the provisions set out in Section 5.2.5.2 of By-law 2009-141, a minimum of 9.5 square metres of amenity area per dwelling unit is permitted in an unconsolidated form in the 'Residential Multiple Dwelling Second Density – Special Provision No. XXX' (RM2)(SP-XXX) zone.
10. **THAT** notwithstanding the provisions set out in Section 5.2.5.2 of By-law 2009-141, a minimum of 126.0 square metres of consolidated amenity area at grade is required in the 'Residential Multiple Dwelling Second Density – Special Provision No. XXX' (RM2)(SP-XXX) zone.
11. **THAT** notwithstanding the provisions set out in Section 5.2.5.1 c) of By-law 2009-141, a maximum density of 82 units per hectare is permitted in the 'Residential Multiple Dwelling Second Density – Special Provision No. XXX' (RM2)(SP-XXX) zone.
12. **THAT** notwithstanding the provisions set out in Section 5.3.5.5 (a) of By-law 2009-141, in-ground waste receptacles shall be permitted in the 'Residential Multiple Dwelling Second Density – Special Provision No. XXX' (RM2)(SP-XXX) zone.
13. **THAT** lands zoned 'Residential Multiple Dwelling Second Density - Special Provision No. XXX' (RM2)(SP-XXX) shall be developed generally in accordance with the Conceptual Site Plan attached as Schedule "B", as it relates to building height, placement and setbacks, as well as the location and configuration of landscape strips, driveways, drive aisles and parking areas.
14. **THAT** the remaining provisions of By-law 2009-141, as amended from time to time, applicable to the above-described lands generally shown on Schedule "A" to this Bylaw, shall apply to the said lands except as varied by this By-law.
15. **THAT** this By-law shall come into force and effect immediately upon the final passing thereof.

READ a first and second time this ____ day of ____, 2024.

READ a third time and finally passed this ____ day of ____, 2024.

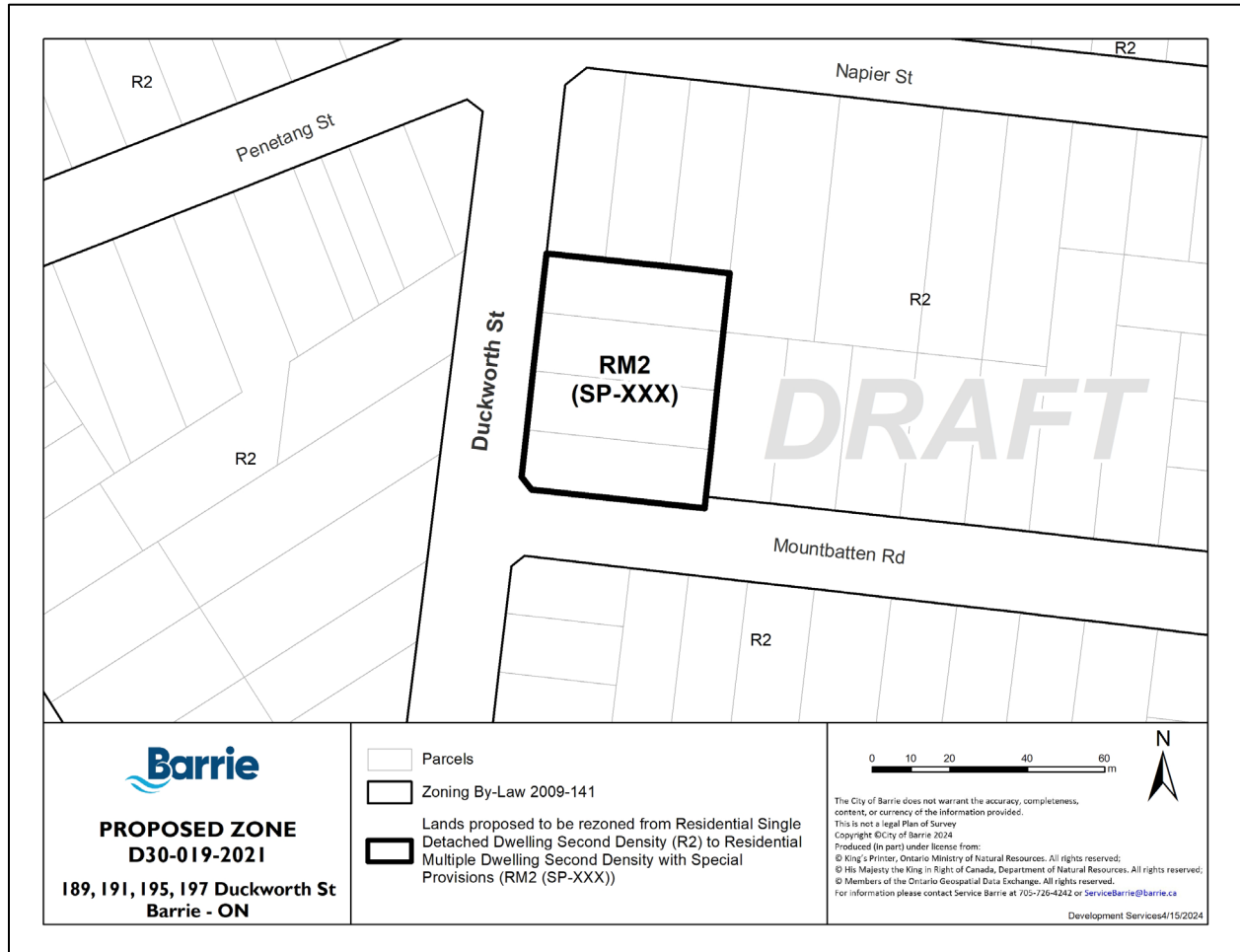


THE CORPORATION OF THE CITY OF BARRIE

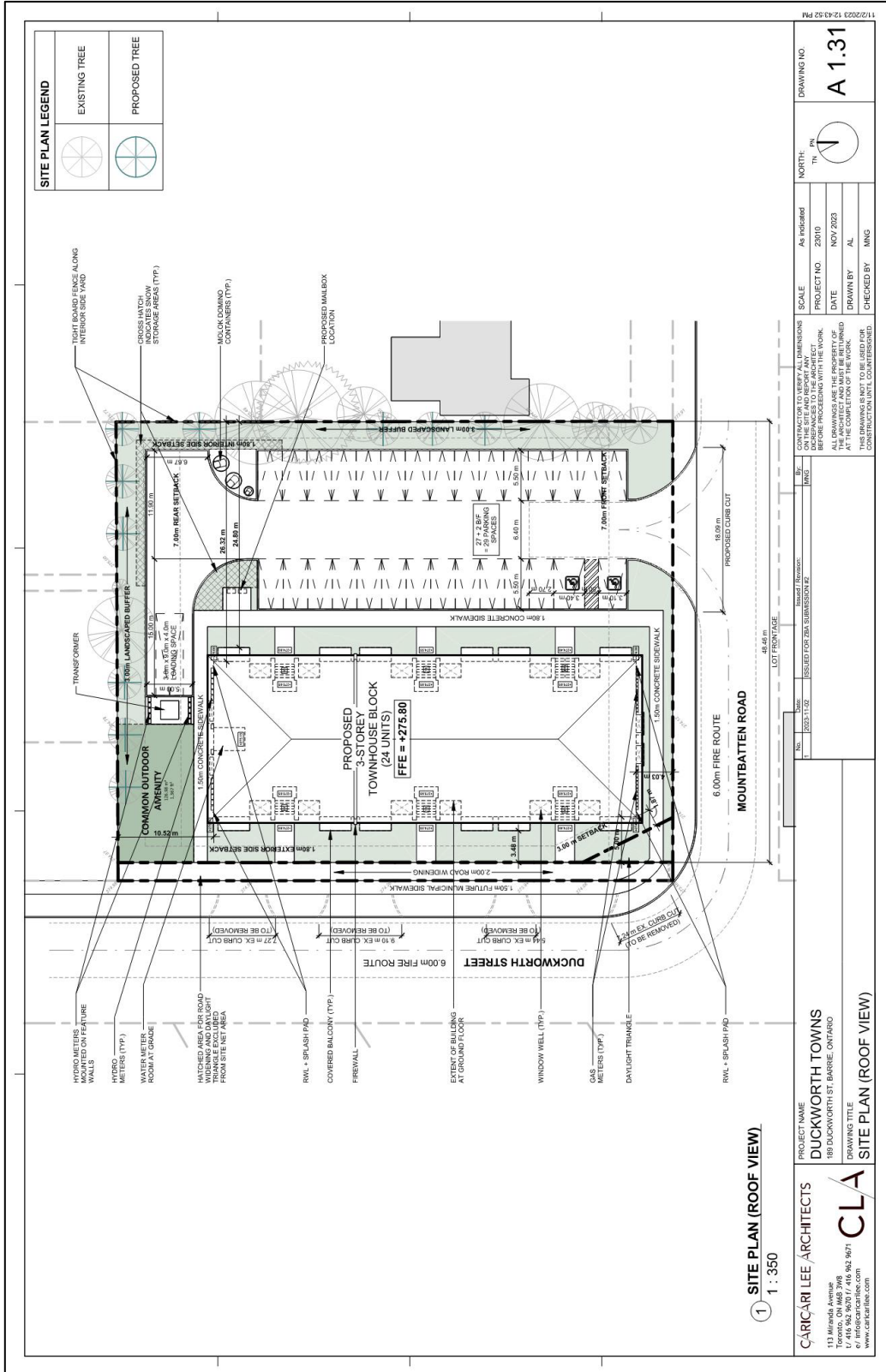
MAYOR – A. NUTTALL

CITY CLERK – WENDY COOKE

Schedule "A" to attached By-law 2024-XXX



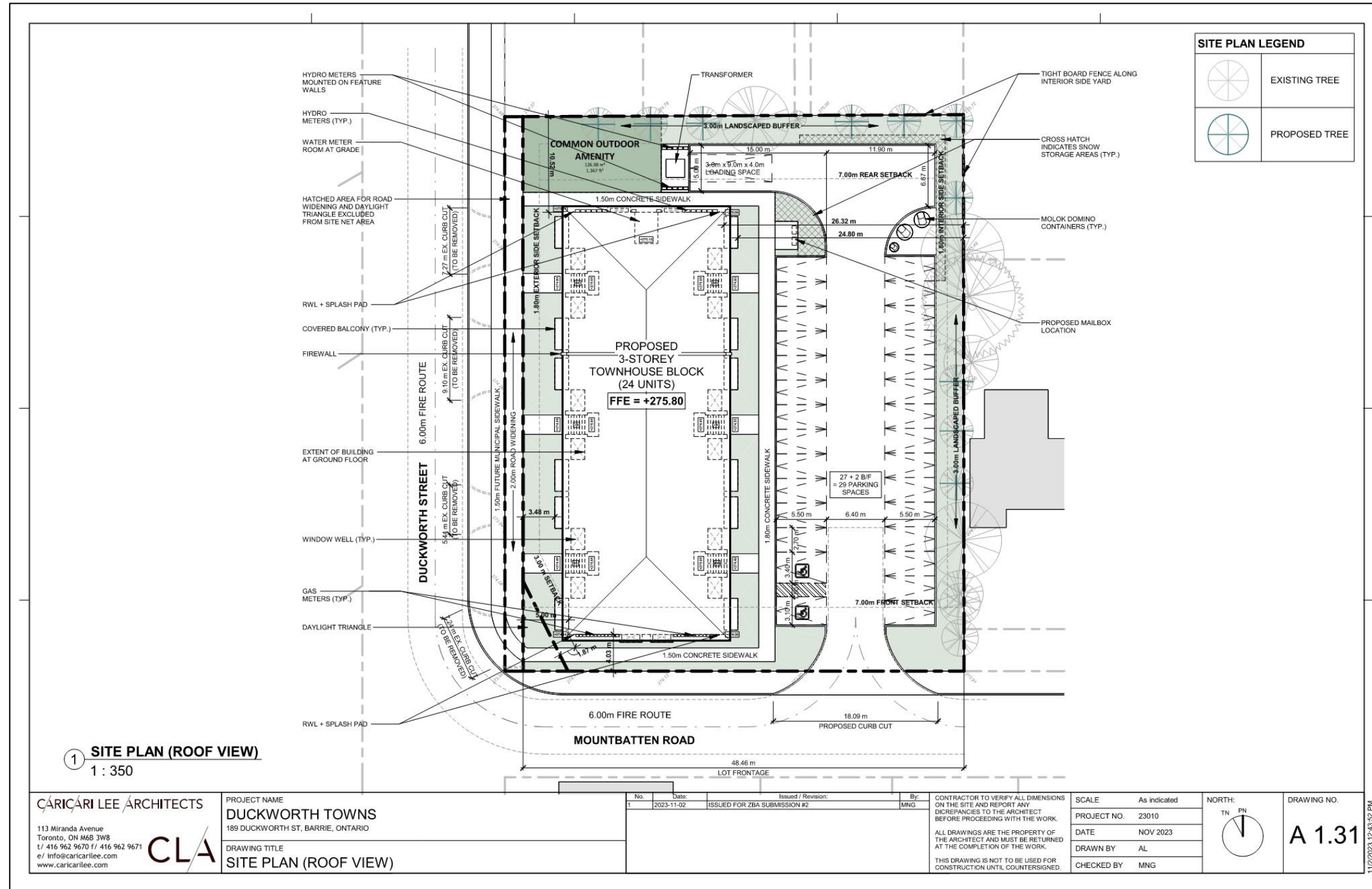
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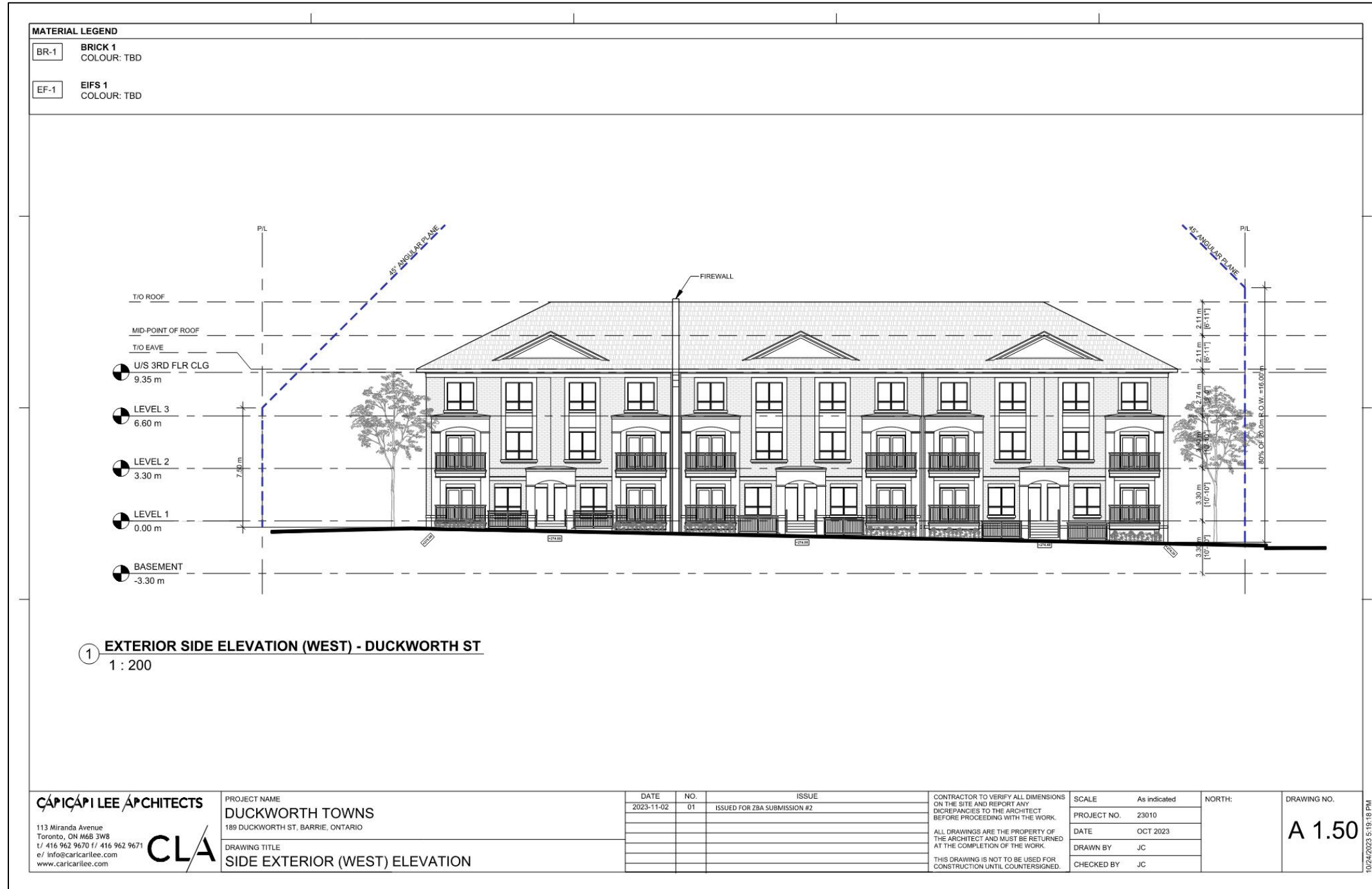
1 SITE PLAN (ROOF VIEW)
1 : 350

CARICARI LEE ARCHITECTS 113 Miranda Avenue Toronto, ON M6S 2W8 T: 416-593-8671 E: info@caricari-lee.com www.caricari-lee.com		PROJECT NAME DUCKWORTH TOWNS 189 DUCKWORTH ST. BARRIE, ONTARIO		DRAWING TITLE SITE PLAN (ROOF VIEW)	
PROJECT NO. 23010 DATE NOV 2023 DRAWN BY AL CHECKED BY MNG		SCALE As Indicated PROJECT NO. 23010 DATE NOV 2023 DRAWN BY AL CHECKED BY MNG		DRAWING NO. A 1.31	
CONTRACTOR TO VERIFY ALL DIMENSIONS DIMENSIONS TO THE ARCHITECT ALL DIMENSIONS ARE THE PROPERTY OF THE ARCHITECT AND MUST BE RETURNED TO THE ARCHITECT WITHIN THE TIME PERIOD SPECIFIED IN THE DRAWING INTL. CONTRACTOR'S CONSTRUCTION INTL. CONTRACTOR'S					

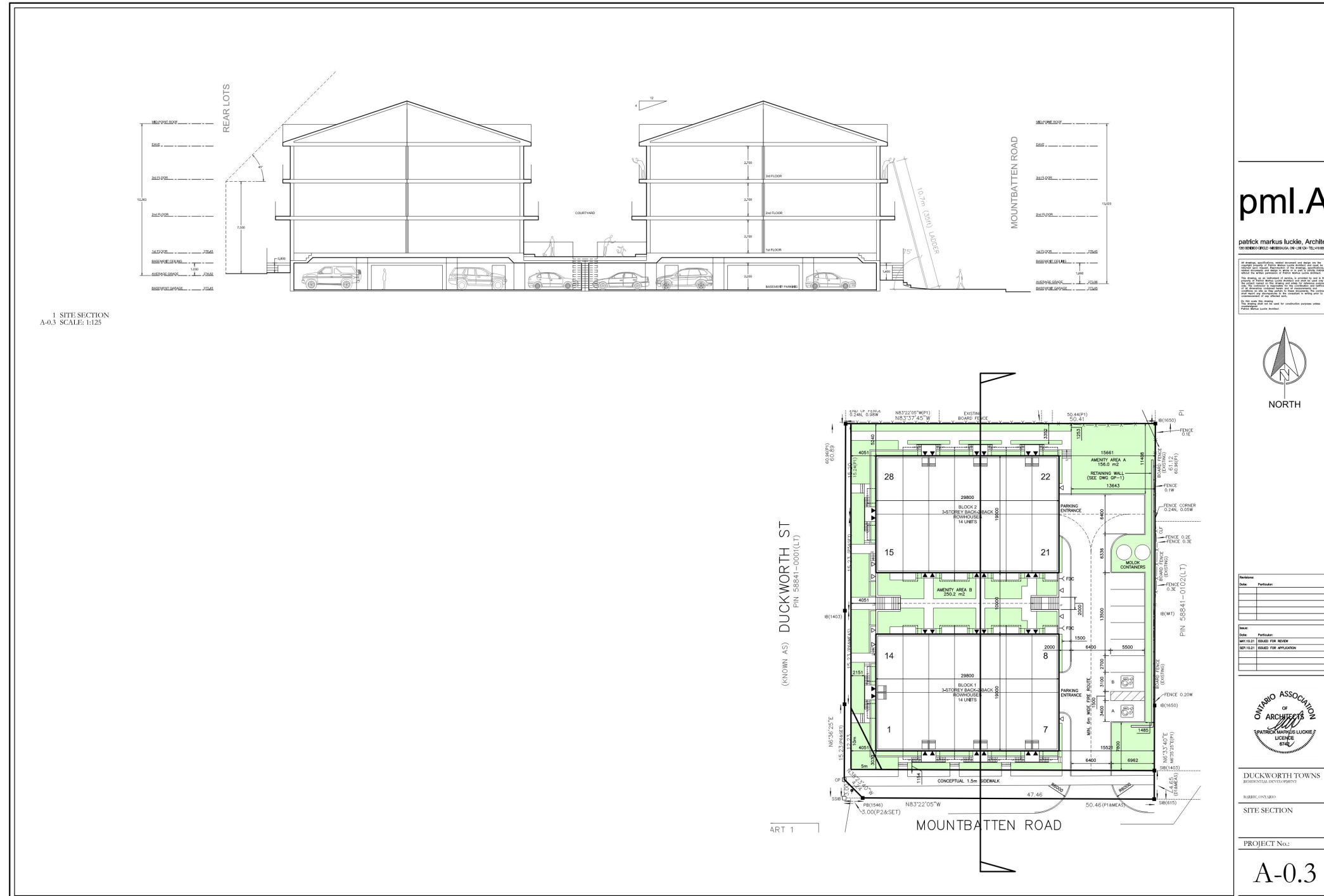
APPENDIX "B"
Conceptual Site Plan



APPENDIX "C"
Proposed Concept Building Elevations



APPENDIX "D"
Previous Concept Plan (2022)



pml.A
patrick markus luckie, Architect
230 BAYVIEW AVE. SUITE 200, SCARBOROUGH, ONT. M1W 2Y1
416-291-1111

NORTH

Revision	Date	Particulars



DUCKWORTH TOWNS
MUNICIPALITY

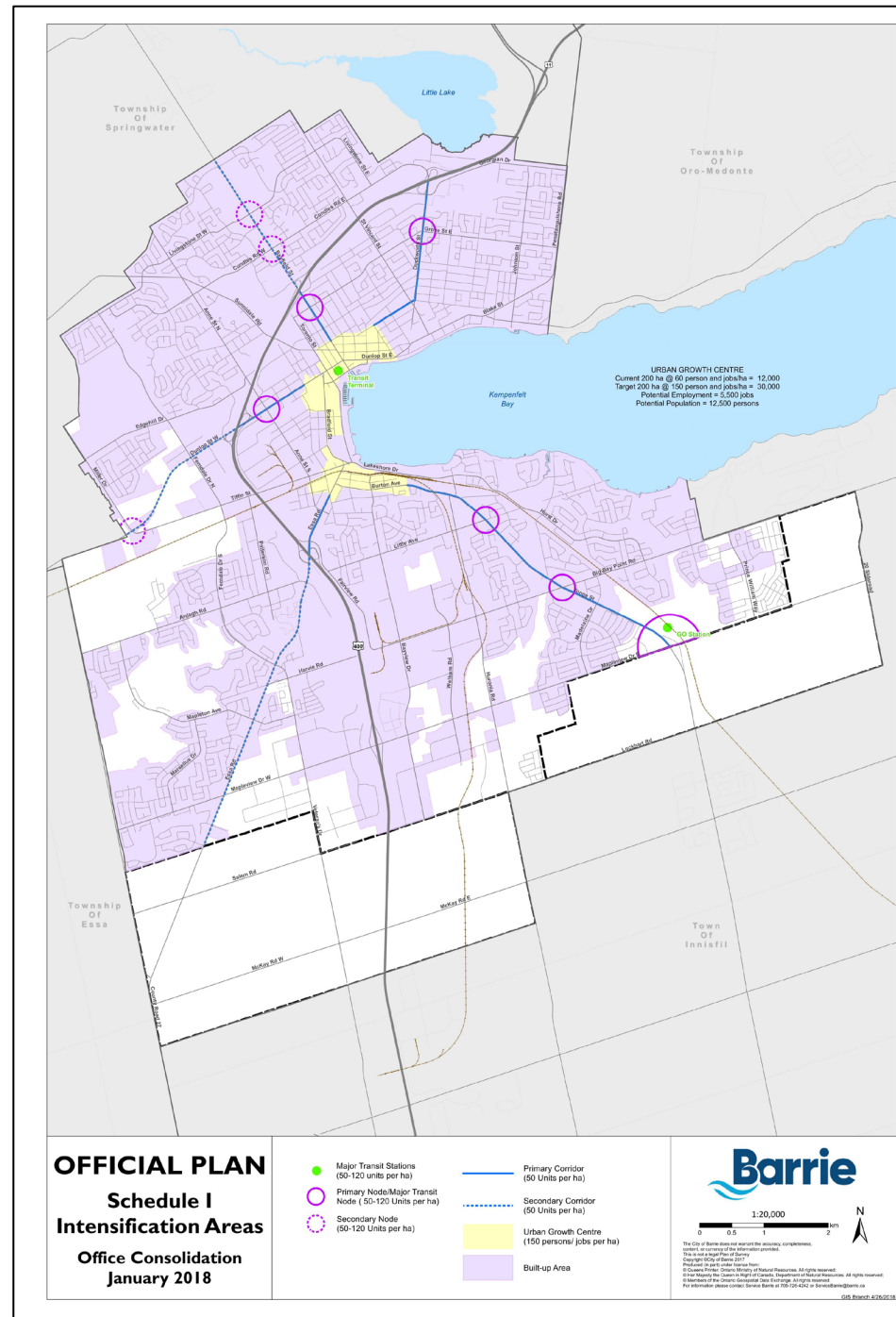
BARRE, ONTARIO

SITE SECTION

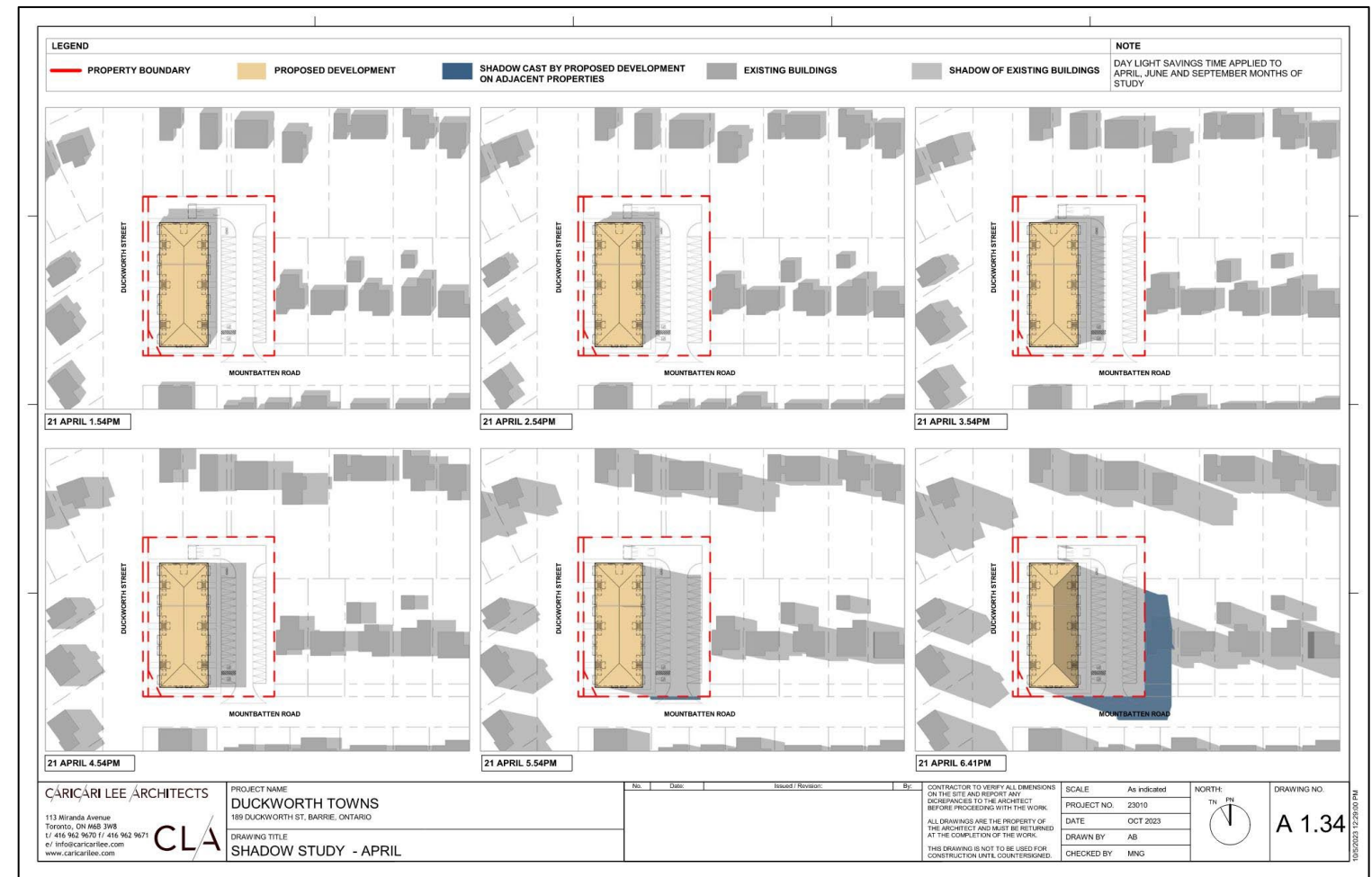
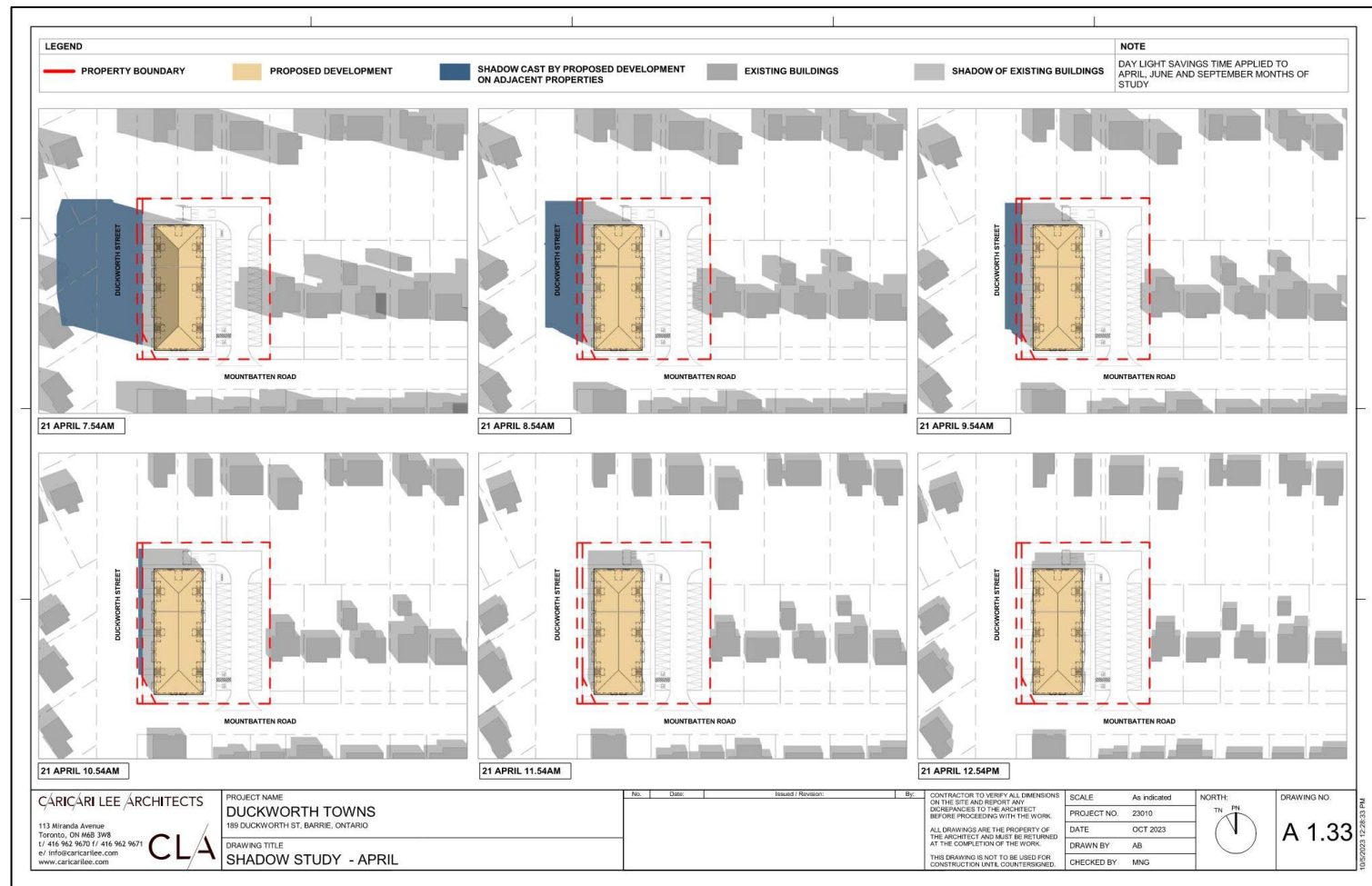
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A-0.3

APPENDIX "E"

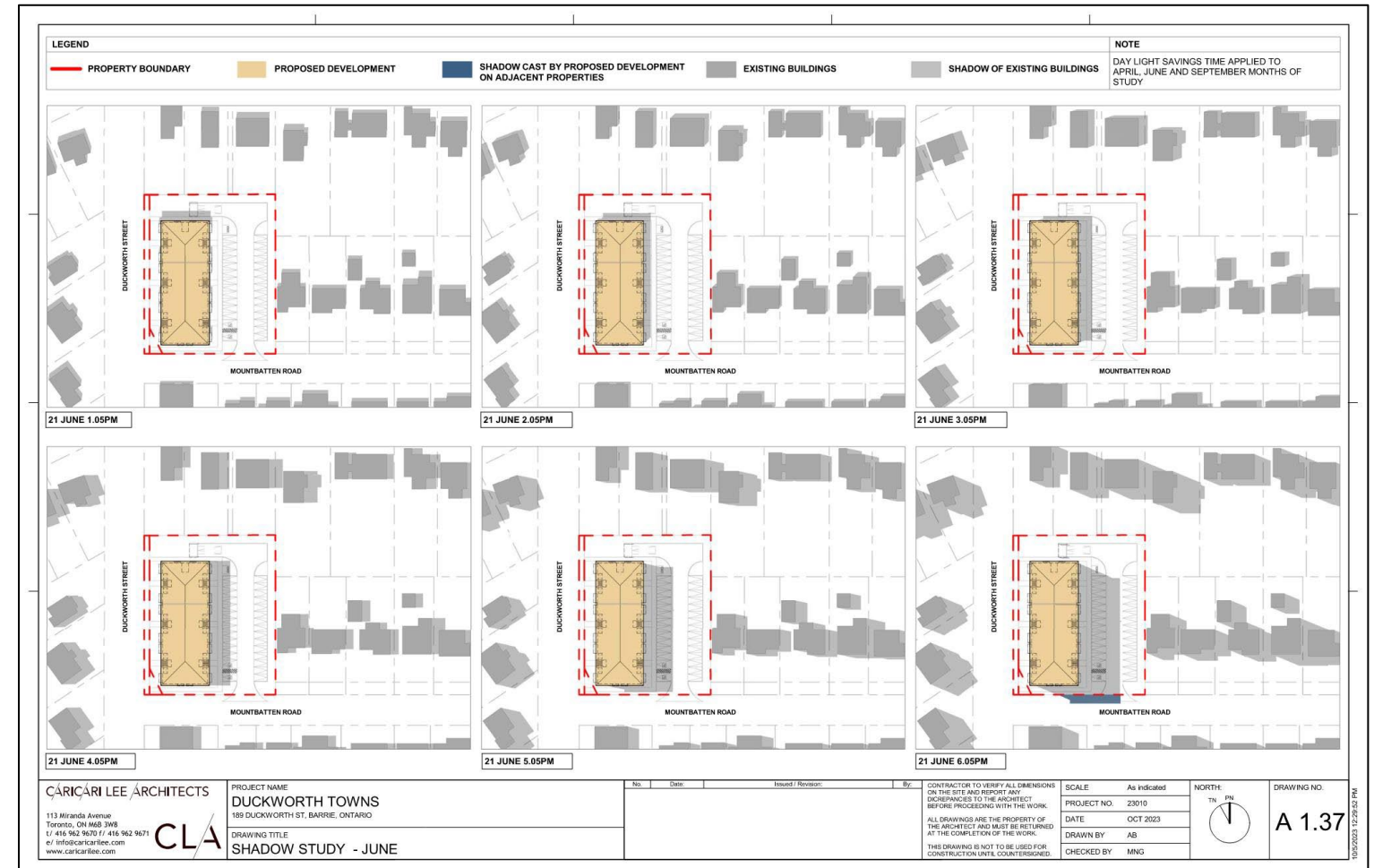
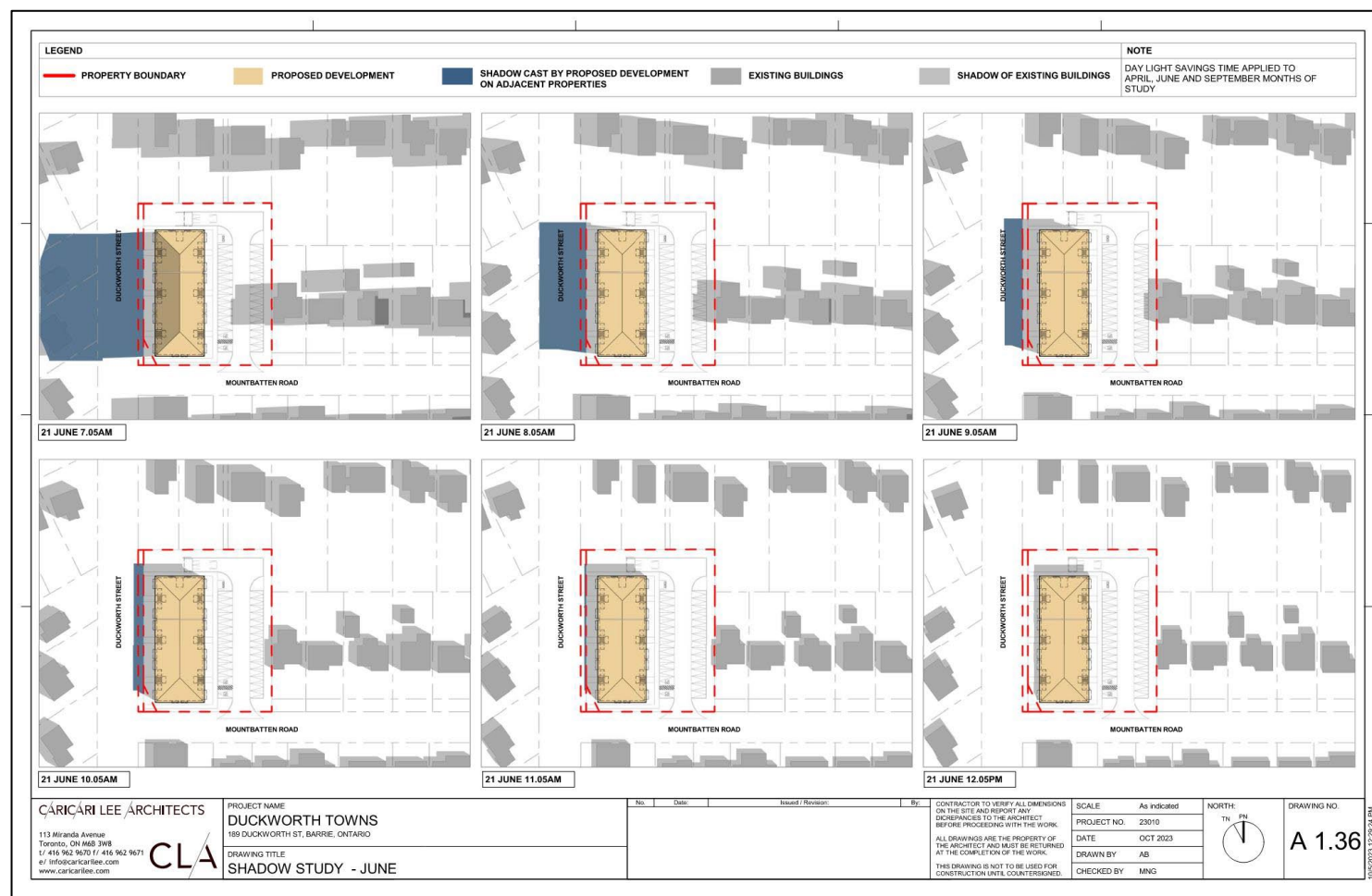
Schedule "I" - Intensification Areas (Official Plan 2010)



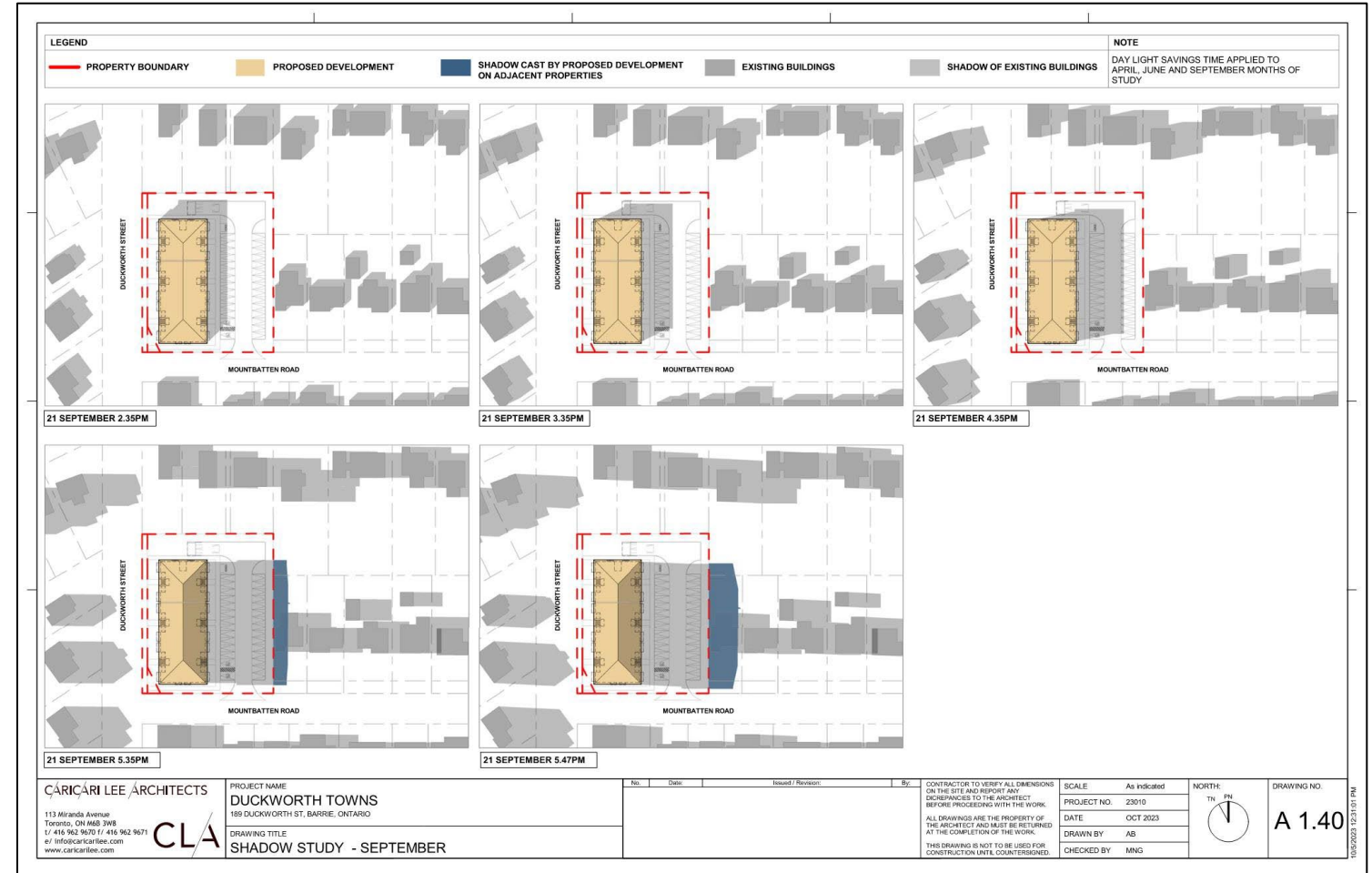
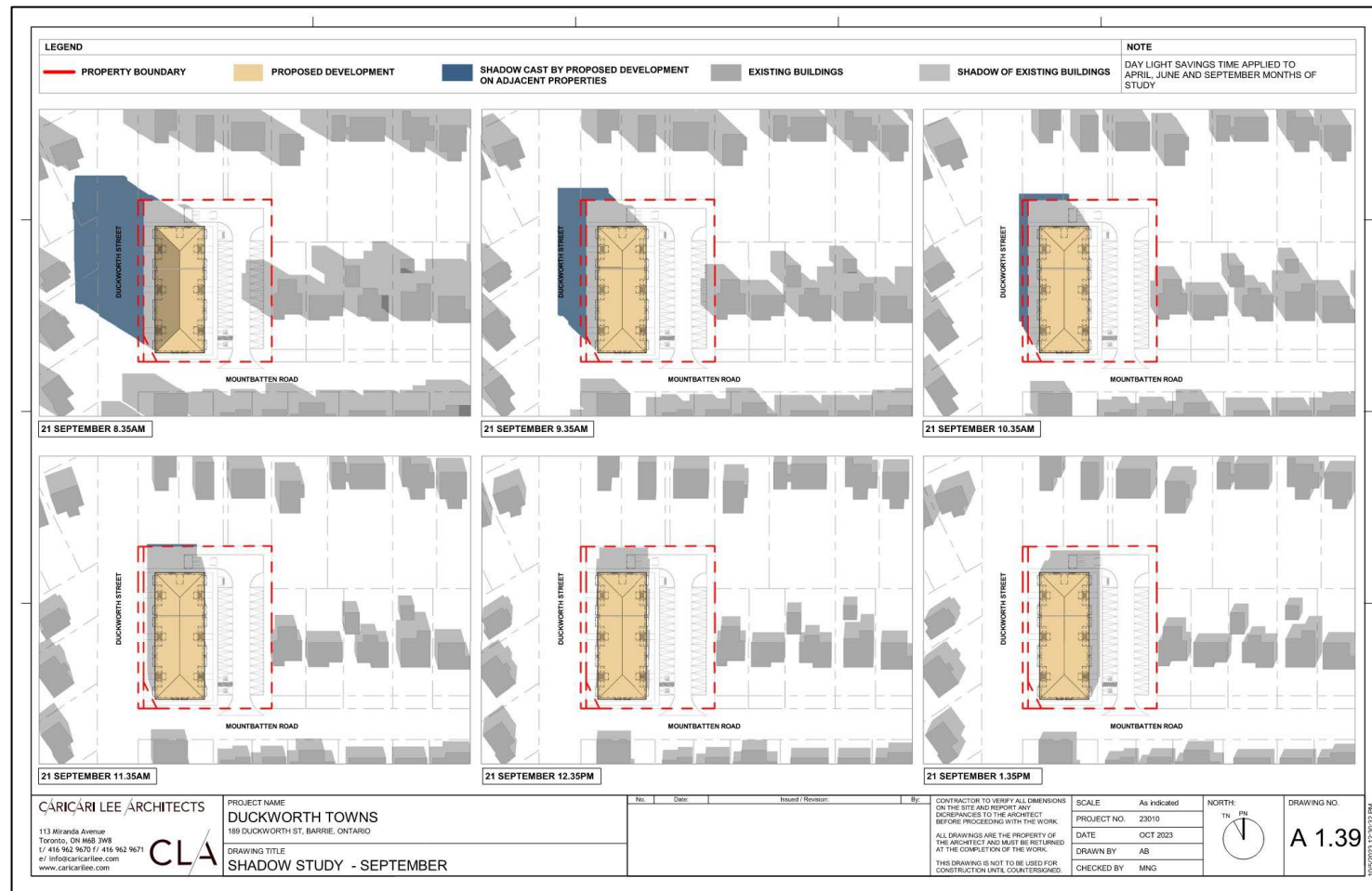
APPENDIX "F"
Four Season Shadow Study
Shadow Study (April)



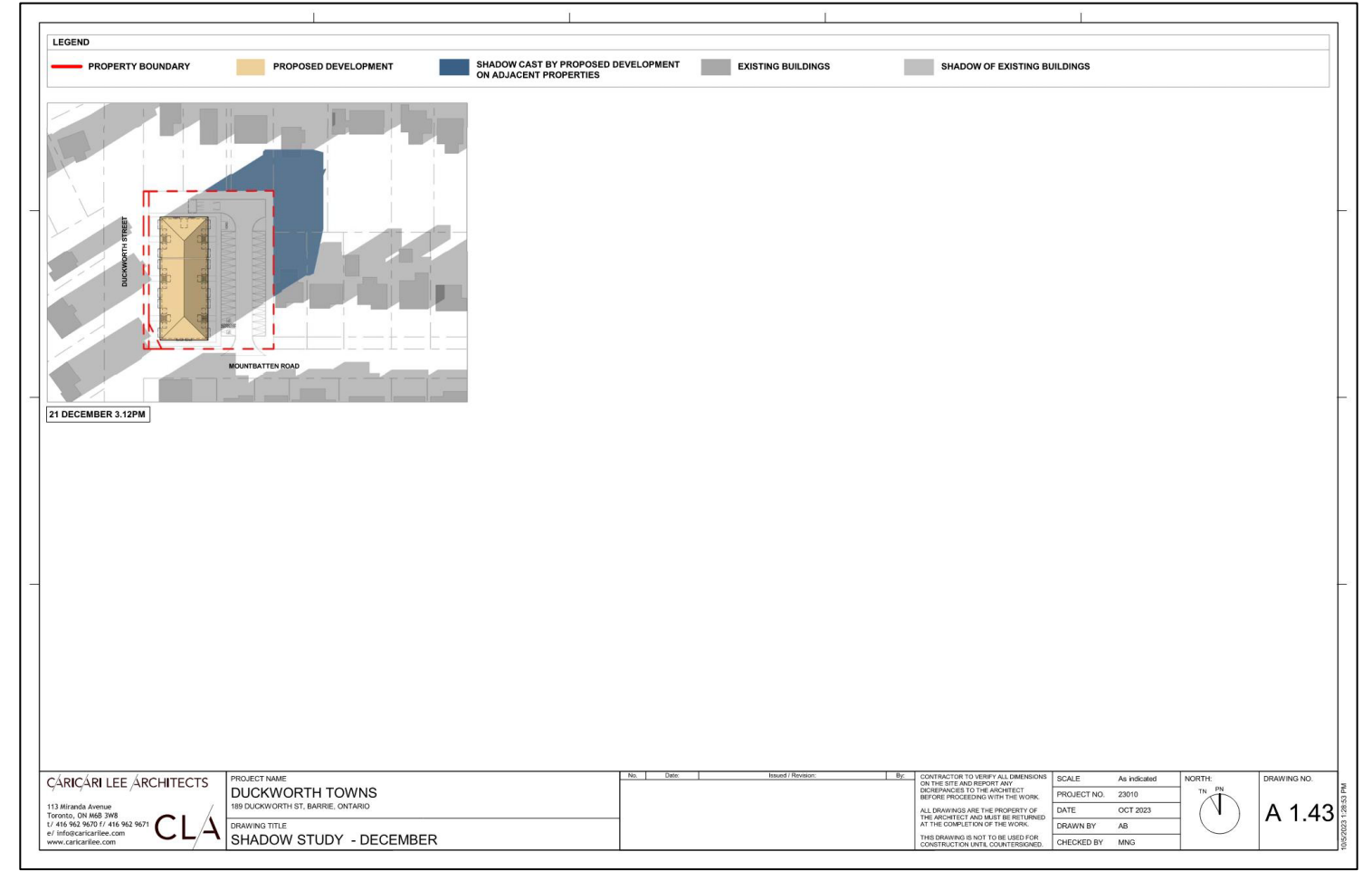
APPENDIX "F"
Shadow Study (June)



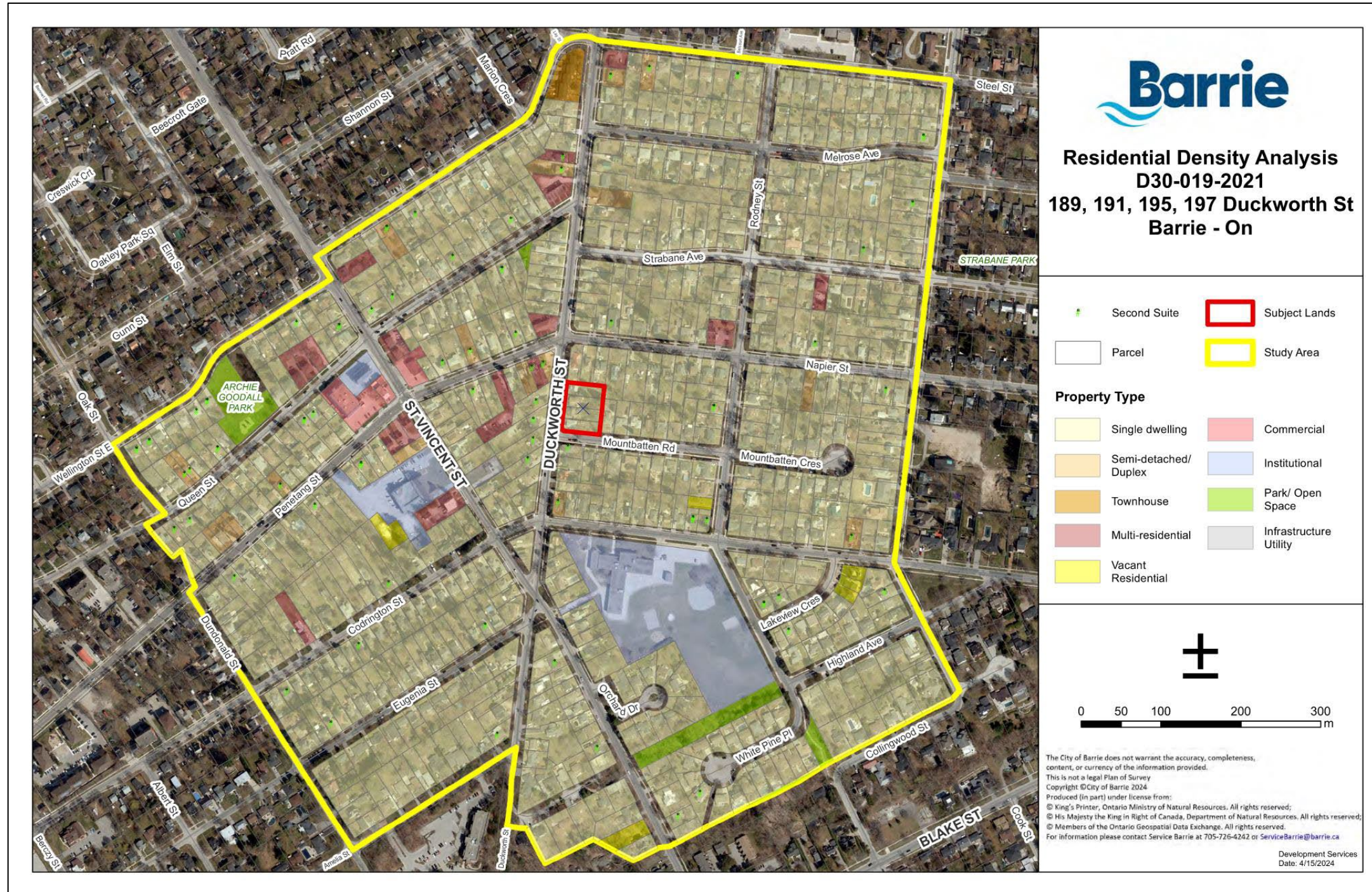
APPENDIX "F"
Shadow Study (September)



APPENDIX "F"
Shadow Study (December)



APPENDIX "G"
Residential Density Analysis



APPENDIX "G"

Residential Density Analysis

RESIDENTIAL DENSITY ANALYSIS
FILE No. D30-019-2021
189, 191, 195, 197 DUCKWORTH ST, BARRIE - ON

Total Study Area	75.08 ha
Total Developable Area - <i>Only residential</i> (Private properties)	51.41 ha
Total Developable Area - <i>All Residential and Non-Residential</i> (Private properties)	55.97 ha
Total Area Parks/ Open Space/SWM Ponds/Walkways/Laneways	1.42 ha
Total Area Roads right of way	17.70 ha

PROPERTY USAGE TYPE	Total Dwelling Units	Land Area (ha)	Residential Density Dwelling Units/ha
Single Dwelling Unit	668	47.94	13.93
Semis/ Duplex	38	1.23	30.90
Townhouse Dwelling Unit	9	0.22	40.56
Multiresidential	79	1.62	48.73
Vacant Residential		0.39	
Non- residential areas(Commercial/ Institutional)		4.56	
Parks/ Open Space/ Walkway / Laneway		1.42	
Subject Property 189, 191, 195, 197 Duckworth St - File No D30-019-2021 <i>The proposed development consists of a three storey stacked townhouse development containing 24 units.</i>	24	0.29	82.62
Current Residential Density <i>(Only Residential Lands included)</i>	798	51.41	15.52
Current Residential Density <i>(All Residential and Non- residential lands included)</i>	798	55.97	14.26
Projected Residential Density Including Proposal for Subject Lands <i>(Only Residential Lands included)</i>	818	51.41	15.91
Projected Residential Density Including Proposal for Subject Lands <i>(All Residential and Non- residential lands included)</i>	818	55.97	14.62

Prepared by: Development Services
Date: April 15, 2024

Note:

This Density Analysis is based on the Assessment Database.

MPAC property Code was used to identify the number of residential units in the Area.

Second Suite units registered in the area were also added; they were counted on their corresponding dwelling unit.

Areas for Parks/ Open Space/ SWM Ponds/ Roads and Laneways were not included in the density calculations.

Any residential units existing in the Subject Lands have been included in the analysis; adding them for current density calculation and subtracting them for the Projected Densities as needed.