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**TO:** **PLANNING COMMITTEE**

**SUBJECT:** **SITE PLAN CONTROL REQUIREMENTS FOR PROCESSING FOR PHASE 1 OF 51 – 75 BRADFORD STREET AND 20 CHECKLEY STREET (BARRIE LAKESHORE DEVELOPMENTS INC.)**

**WARD:** **2**

**PREPARED BY AND KEY CONTACT:** **C. KITSEMETRY, RPP, SENIOR PLANNER  
EXT. #4430**

**SUBMITTED BY:** **M. BANFIELD, RPP  
DIRECTOR OF DEVELOPMENT SERVICES**

**GENERAL MANAGER APPROVAL:** **A. MILLER, RPP  
GENERAL MANAGER OF INFRASTRUCTURE AND GROWTH  
MANAGEMENT**

**CHIEF ADMINISTRATIVE OFFICER APPROVAL:** **M. PROWSE, CHIEF ADMINISTRATIVE OFFICER**

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### **RECOMMENDED MOTION**

1. That the Site Plan Control Application submitted by SmartCentres, on behalf of Barrie Lakeshore Developments Inc. (2714708 Ontario Inc. & Greenwin Barrie Inc.) for Phase 1 of the development on lands known municipally as 51-75 Bradford Street and 20 Checkley Street, be approved in principle per the Site Plan Control Requirements for Processing outlined in Appendix "A" to Staff Report DEV016-21.
2. That staff maintain the delegated responsibility for final clearance of the Site Plan Control Requirements for Processing that culminate in the preparation and registration of a Site Plan Agreement for the project including, but not limited to, a Final Plan package for Phase 1.

### **PURPOSE & BACKGROUND**

3. The purpose of this report is to facilitate the release of Site Plan Control Requirements for Processing for Phase 1 of the development proposed on the lands known as 51-75 Bradford Street & 20 Checkley Street. The draft requirements are included as Appendix "A" to Staff Report DEV016-21. As part of Council Adoption of Direction 20-P-049 on January 11, 2021, regarding the Official Plan Amendment (By-law 2021-009) and site specific Zoning By-law Amendment (By-law 2021-010) for this property, there was a requirement for Site Plan Control to be "bumped up" to Council for approval.
4. Phase 1 of the project by Barrie Lakeshore Developments Inc. consists of:
  - Podium and Tower consisting of 25 storeys (85 metres) in height;
  - Hotel with 145 Suites;
  - Apartment with 233 Rental Units;
  - Restaurant and Amenities;
  - Environmental Protection Area; and,
  - Temporary parking lot on that portion of the property lands to be developed as Phase 2.

5. Staff and partner agencies have reviewed the Site Plan Application for Phase 1 in accordance with current legislative requirements, standards, and design guidelines, and have determined that the latest submission is acceptable to move forward with finalizing the infrastructure and design details. Future phases of this development will return to Council for site plan approval as the project progresses.

#### Site and Location

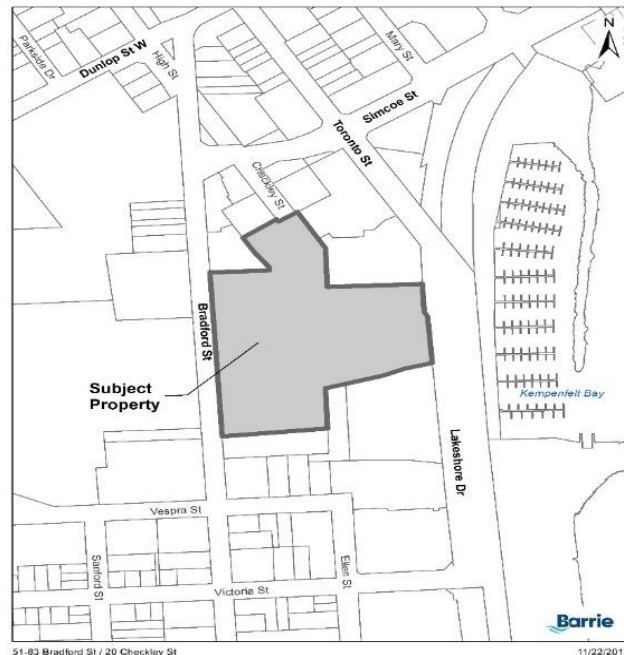
6. The lands subject to the application include the properties known municipally as 51 – 75 Bradford Street and 20 Checkley Street. The site is approximately 3.5 hectares in size and has frontage on both Bradford Street and Lakeshore Drive, in addition to an access to Simcoe Street via Checkley Street. Bunker's Creek and the Bunker's Creek Ecopark provide the south boundary to this development proposal.

**North:** Residential apartments (2 & 6 Toronto Street – 16 storeys), Simcoe Street, office, and commercial uses

**East:** Lakeshore Drive, municipal parking lot, City Marina and Kempenfelt Bay

**South:** Bunker's Creek, residential apartments (Nautica – 16 storeys), commercial uses

**West:** Bradford Street, former Barrie Central Collegiate (currently under consideration for development by HIP Barrie Central Inc.)



#### ANALYSIS

##### Zoning

7. Staff have confirmed that the site plan as proposed meets the standards set out in site specific By-law 2021-10 for the C1-2 (SP-602) (H-151) zone, as well the standards of Comprehensive Zoning By-law 2009-141 as applicable. The Zoning Matrix is attached as Appendix "C" to Staff Report DEV016-21.
8. The current zone includes Hold provision (H-151). The Hold identifies that the following items must be completed to the satisfaction of the Director of Development Services:
  - That the owner/applicant undertake and/or agree to satisfy the requirements of a Certificate of Property Use and/or Record of Site Condition under the Environmental Protection Act prior to any site works or issuance of a Building Permit;
  - Approval of a Site Plan Control application and execution of a Site Plan Agreement; and,
  - Confirmation and refinement of the lands zoned Environmental Protection (EP) for future dedication to the City of Barrie.

9. Staff can confirm that progress has been made to satisfy the Hold provisions as follows:
- The owner/applicant is in the process of submitting a Risk Assessment to the Ministry of Environment, Conservation and Parks (MECP) for review, which is the first step in the approvals process for a Record of Site Condition;
  - As the purpose of Staff Report DEV016-21 is to provide Site Plan Control Requirements for Processing, this report highlights that staff and the applicant are working diligently to finalize the proposed building and site design for construction; and,
  - The dedication of EP lands to the City will be completed as part of the Site Plan Agreement process for Phase 1.
10. A By-law to remove the Hold provisions, in whole or in part, will be brought forward to Council when the Director of Development Services confirms that the requirements have been completed to their satisfaction. This will not happen until at least the fall of 2021 and will need to be done prior to the execution of the site plan agreement for Phase 1.

#### Site Plan Review

11. Key attributes of Site Plan Control that have been investigated in detail by staff include:
- **Planning and Urban Design** for the podium and tower structure location, pedestrian circulation, and public interface.
  - **Municipal Infrastructure** connections and in site service design including water, sewer, and stormwater management.
  - **Transportation** review for the proposed increase in traffic, access connections from Lakeshore Drive, Checkley Street, and the internal automotive and pedestrian traffic circulation.
  - **Bunker's Creek Environmental Protection Area** delineation, mitigation, naturalization, and a trail connection from Bradford Street to Lakeshore Drive.
  - **Source Water Protection and Environmental Compliance** requirements for contaminant management, dewatering, historic waste assessment in coordination with the Record of Site Condition, and foundation depth as it pertains to protecting the municipal supply aquifer.
  - **Landscaping** for the site, integration of public-private spaces, and streetscape, including screening of temporary parking lot on Phase 2.

#### Next Steps

12. The release of the Requirements for Processing provides the opportunity to have the comments and conditions of staff more formally acknowledged by all parties and provides direction for the remaining steps of the Site Plan Control process. Next steps would include finalizing the site and building design, confirmation of the Phase 1 site servicing, foundation depth, contaminant management plan details, landscaping and dedication of the Environmental Protection lands, road access and widening dedications, calculation of fees and securities, and registration of a Site Plan Agreement for Phase 1.

#### **ENVIRONMENTAL AND CLIMATE CHANGE IMPACT MATTERS**

13. A significant portion of the site has been identified as floodplain and subsequently zoned and designated to reflect the floodplain limit, Bunker's Creek watercourse and the contributing feature area. In addition to coordinating the project details with the ongoing capital project for the rehabilitation of Bunker's Creek, re-naturalization efforts and a trail linkage from Bradford Street to Lakeshore Drive have been included in this project.

14. As part of the final plan submission, the applicant will provide confirmation that the design reflects the recommendations from the Energy Conservation Report (EQ Building Performance, August 5, 2020) submitted in support of the Official Plan and Zoning By-law Amendment application. Some specific building options include energy efficient servicing, low impact finish and glazing choices, and green roof design.

## **ALTERNATIVES**

15. The following alternative is available for consideration by Planning Committee:

**Alternative #1** Planning Committee could refer the proposed Site Plan Control Requirements for Processing back to staff for additional consultation with the owner/applicant and the commenting team.

This alternative is not recommended as staff and partner agencies have been working cooperatively with SmartCentres on behalf of Barrie Lakeshore Developments Inc. to address the comments and technical consideration for the ultimate design of Phase 1 of this development. As noted previously, the release of the Requirements for Processing provides the opportunity to have the comments and conditions of staff more formally acknowledged by all parties and provides direction to formally complete the remaining steps of the Site Plan Control process.

## **FINANCIAL**

16. All costs associated with the approval and development of the site would be the applicant's responsibility. The applicant would be responsible for all capital costs for any new infrastructure required within the development limits and any of the frontage costs associated with upsizing to municipal water and sewer mains already installed, if required.
17. The current municipal tax revenue from the property known municipally as 51-75 Bradford St. & 20 Checkley St. is \$94,994.46. The estimated municipal property tax increase would include approximately \$453,418.00 for the 233 rental apartment units and \$122,815.00 for the 145 hotel suites plus amenities. Therefore, based on the 2020 tax rate, there would be an increase in municipal tax revenue to \$576,233.00. (2020 rate)
18. A summary of the Development Charges calculated for Phase 1 of this development is provided in the chart below:

D11-020-2020 - Phase 1 DC Estimate				
Rates frozen as of Site Plan Application Date: September 8, 2020				
Indexing Rate: 3.94%				
Development Charge Calculations	Rate	Metric	Amount	Total Invoiced
Development Charges - Bachelor and 1 Bedroom	\$25,934.00	Per Unit	62.00	\$ 1,607,908.00
Development Charges - Apartments 2+ Bedrooms	\$36,943.00	Per Unit	161.00	\$ 5,947,823.00
Development Charges - Hotel	\$ 214.67	Square Meter	8,432.60	\$ 1,810,226.24
Development Charges - Hotel Indoor Amenity Space	\$ 214.67	Square Meter	1,117.97	\$ 239,994.62
Education Levies - Residential	\$ 3,559.00	Per Unit	233.00	\$ 829,247.00
Education Levies - Non-Residential	\$ 0.55	Square Foot	102,801.38	\$ 56,540.76
Cash In Lieu of Parkland	\$ 5,726.00	Per Unit	233.00	\$ 1,334,158.00
Cash In Lieu of Parkland (5% Appraised Land Value)	5%	Appraised Land Value	0.00	Need appraisal
Finance Administration Fee	\$ 80.00	Per Unit	233.00	\$ 18,640.00
Crystallized Rate Indexing (Estimated Interest as of May 10, 2021)				\$ 253,007.61
Total Estimate not including non-residential Cash in Lieu of Parkland				\$12,097,545.23



19. Building permit application fees as an average are estimated to be in the order of \$660,406.31. (2021 rate, subject to an annual inflation adjustment on January 1<sup>st</sup> of each year)

**LINKAGE TO 2018–2022 STRATEGIC PLAN**

20. The recommendation(s) included in this Staff Report support the following goals identified in the 2018-2022 Strategic Plan:

- ☒ Fostering a Safe and Healthy City
  - Build a greener Barrie while mitigating and adapting to climate change
- ☒ Building Strong Neighbourhoods
  - Build walkable, diverse neighbourhoods that encourage community connections
- ☒ Improving the Ability to Get Around
  - Increase transportation options, including active transportation modes
  - Make connections

21. In accordance with Council's goals, the proposed development would provide for a high density built form with a hotel and rental residential units that will utilize existing services and infrastructure. The proposed development promotes and facilitates community connections to the waterfront, supports active transportation and public transit, and will contribute to the overall commercial success of the downtown by adding residents and a hotel use.

Attachments: Appendix "A" – Draft Site Plan Control Requirements for Processing  
Appendix "B" – Proposed Site Plan & 3D Perspectives  
Appendix "C" – Zoning Matrix

## APPENDIX "A"

### Draft Site Plan Control Requirements for Processing

*File: D11-020-2020, 51-75 Bradford Street & 20 Checkley Street*

#### **SITE PLAN CONTROL REQUIREMENTS FOR PROCESSING**

Section 41(13) b of the *Planning Act* allows Council to delegate by By-law Council's authority to approve site plans to an appointed officer of the Municipality.

Council By-law 99-312, as amended, has delegated Site Plan Approval authority to the Director of Development Services, Manager of Growth and Development and Manager of Strategic Initiatives, Policy and Analysis. This authority permits the appointed officer(s) to recommend that the City Clerk prepare site plan agreements for execution and registration on title.

The appointed officer hereby grants Preliminary Approval to Site Plan Application File **D11-020-2020** located at Part of Broken Lot 25, Concession 5, Vespra, Part of East Part of 24, Concession 5, Vespra, Lots 115, 116, 117, 118, 119, 120 and 121, inclusive, East Side of Bradford Street, all of Bob Street on Robert Ross Unregistered Plan, Lot 90 West Side of High Street, Part of Lot 65 East Side of High Street and Part of High Street on Registered Plan 115, Part of Ellen Street (Closed By-Law as in SC450705) on Registered Plan 22, being designated as Parts 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17 and 18, inclusive, on Reference Plan 51R-37947, City of Barrie, known municipally as **51-75 BRADFORD STREET & 20 CHECKLEY STREET** on lands owned by **GREENWIN BARRIE INC. & 2714708 ONTARIO INC.** upon registration of the development agreement for the above noted property to be prepared and completed in accordance with the following requirements:

This approval shall relate to the following plans as amended, if necessary:

	<b>Designer/Architect</b>	<b>Plan No.</b>	<b>Date/Rev.</b>
a) Site Plan	TFAI	SPA001-SPA156	Apr. 16, 2021 REV.#4
b) Building Elevations	TFAI	SPA301-SPA304	Apr. 16, 2021 REV.#4
c) Building Cross Sections	TFAI	SPA401-SPA402	Apr. 16, 2021 REV.#4
d) 3D Perspectives	TFAI	SPA801-SPA803	Apr. 16, 2021 REV.#4
e) Material Board	TFAI	SPA805	Apr. 16, 2021 REV.#4
f) Grading Plan	SCS	GR-1	June 1, 2021 REV.#6
g) Grading Plan	SCS	GR-2 GR-3	Apr. 16, 2021 REV.#3
h) Details Plan	SCS	D-1	Apr. 16, 2021 REV.#3
i) Removals Plan	SCS	R-1	Apr. 16, 2021 REV.#3
j) ESC Plan	SCS	ESC-1 ESC-2	Apr. 16, 2021 REV.#3
k) Servicing Plan	SCS	S-1	June 1, 2021 REV.#6
l) Landscape Plan	MHBC	L-1	May 27, 2021 REV.#5
m) Layout Plan	MHBC	L-1.1	May 27, 2021 REV.#5
n) Tree Planting Plan	MHBC	L-1.2 L-1.2.1	May 27, 2021 REV.#5
o) Shrub Planting Plan	MHBC	L-1.3 L-1.3.1	May 27, 2021 REV.#5

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p) Restoration Plan West	MHBC	L-1.4	May 27, 2021 REV.#5
q) Restoration Plan East	MHBC	L-1.5	May 27, 2021 REV.#5
r) Landscape Details	MHBC	L-2 L-2.1 - L-2.3	May 27, 2021 REV.#5
s) Tree Inventory and Preservation Plan	Kuntz Forestry	Figure 1	July 22, 2020 REV.#2
t) Sight Distance Plan	BA Group	SD03	Dec. 22, 2020 REV.#2
u) Signage and Pavement Marking Plan	BA Group	SN01	Dec. 22, 2020 REV.#2
v) Electrical Site Plan	ABLE	PH-01	May 20, 2021 REV.#4
w) Electrical Site Plan	ABLE	PH-02	Apr. 15, 2021 REV.#1
x) Crane Swing Plan	LCG	SPA152	Dec. 16, 2020 REV.#1

#### **CONDITIONS**

Prior to the appointed officer recommending that the City Clerk execute the Site Plan Agreement, the following requirements shall be satisfied and/or addressed:

#### **Revisions**

- A. That the plans be amended to reflect the following:
- i) That the site plan drawings be amended as necessary to address the details required to clear the Hold (H-151) from By-law 2021-10 to the satisfaction of the Director of Development Services, including but not limited to, the Owner/Applicant agreeing to implement the requirements of a Certificate of Property Use and/or Record of Site Condition (RSC) under the *Environmental Protection Act* prior to any site works or issuance of a Building Permit.
  - ii) That the site plan drawings be amended as necessary to address the comments from the Development Services – Planning and Urban Design technical requirements and current standards, including but not limited to, Urban Design comments dated May 31, 2021, as attached.
  - iii) That the site plan drawings be amended as necessary to conform to the Development Services - Approvals technical requirements and current standards as identified in their comments dated May 31, 2021, including but not limited to, single service water and sewer service connections, also noted in comments from Water Operations dated May 25, 2021, as attached.
  - iv) That the site plan drawings be amended as necessary to conform to the Development Services – Transportation Planning technical requirements and current standards as identified in their comments dated May 26, 2021, as attached, including but not limited to:
    - a. Road widening dedications on Bradford Street;
    - b. Checkley Street rehabilitation; and,
    - c. Photometrics review of podium fixtures.

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- v) That the site plan drawings be amended as necessary to conform to the Development Services – Parks Planning technical requirements and current standards as identified in their comments dated May 12, 2021, as attached, including but not limited to:
  - a. Structured crossing(s) of stormwater outflow;
  - b. Trail linkage from Bradford Street to Lakeshore Drive; and,
  - c. Native vegetation and planting density.
- vi) That the site plan drawings be amended as necessary, and supporting information submitted, to conform to the technical requirements from Business Performance and Environmental Sustainability as identified in their 3<sup>rd</sup> submission comments dated May 31, 2021, as attached, as well as Sections L. to R. of this document, including but not limited to:
  - a. Foundation depth analysis;
  - b. A Containment Management Plan;
  - c. Dewatering analysis;
  - d. D-4 Assessment, as an addendum to the RSC;
  - e. Fill Management Plan; and
  - f. An update to the Phase 2 ESA to reflect MECP Table 2 standards.
- vii) That the site plan drawings be amended as necessary and supporting information be submitted to conform to the Lake Simcoe Region Conservation Authority (LSRCA) technical requirements and current standards as identified in their 2<sup>nd</sup> submission comments dated February 23, 2021, and March 17, 2021, as attached, as well as Sections S. to W. of this document.

Development Services – Approvals and Transportation Planning

- B. The Owner/Applicant will be required to submit a draft reference plan to reflect any required road widenings and/or daylighting triangles, and environmental protection lands to be conveyed to the City of Barrie at no cost and free of encumbrances. The conveyances shall include:
  - i) The Owner/Applicant dedicate the Environmental Protection lands;
  - ii) That the Owner/Applicant dedicate a road widening of 5.5 metres along the frontage of Bradford Street; and
  - iii) That the Owner/Applicant shall protect a future road widening of 1.5 metres along the entire frontage of Bradford Street and this shall be measured from the future property line.
- C. The Owner/Applicant will be required to provide the following drawings in accordance with the City of Barrie's Site Plan Application Manual and Urban Design Manual:
  - i) Separate site servicing drawing and site grading drawing;
  - ii) Storm drainage catchment drawing (identify/accommodation any external drainage);
  - iii) Sediment and erosion control drawing;
  - iv) Signage and pavement marking drawing in accordance with the Ontario Traffic Manuals. This drawing shall be detailed for use by the paint marking and signage contractor and shall include a legend, details, dimensions and material specifications; and
  - v) Detail and general notes drawing.



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- D. The Owner/Applicant will be required to retain an experienced civil consulting engineer to provide the design, inspection, and certification of the installation of water and sanitary servicing for the proposed development, all to an appropriate connection/outlet. Detailed water servicing requirements are available through Development Services (Approvals).
- E. The Owner/Applicant will be required to retain an experienced civil consulting engineer to provide the design, inspection, and certification of the installation of the storm servicing works including parking lot construction and grading, all to the satisfaction of Development Services (Approvals).
- F. The Owner/Applicant will be required to retain a licensed experienced civil consulting engineer to provide a detailed Stormwater Management Report. The consultant will also be required to obtain, if necessary, Ministry of Environment, Conservation, and Parks (previously Ministry of Environment and Climate Change) approvals for the implementation of any stormwater management works on-site, all to the satisfaction of Development Services (Approvals).
- G. Before any site alteration within the subject property, the Owner/Applicant or his agents will apply for a Site Alteration Permit, as described within By-law 2014-100. Prior to the commencement of any works within the site, all requirements, obligations, and control measures, as described within By-law 2014-100 will be in place and undertaken to the satisfaction of the City of Barrie. Furthermore, it will be the Owner/Applicant's responsibility, through his professional consultant to maintain the said work for the duration of the subject property.
- H. The water distribution system within the limits of this site plan is privately owned and shall be maintained by the Owner/Applicant, and any hydrant installed on-site shall be deemed privately owned. All hydrants have to be maintained as per fire code/insurance requirements.
- I. The Owner/Applicant will be responsible for obtaining a Right-of-Way Activity Permit prior to the commencement of work on the municipal right-of-way.
- J. That the drawings be revised as necessary to reflect the Digital Data Control Requirements. That the drawings be processed in digital format using UTM (Zone 17) NAD83 datum (76 adjustments).
- K. That an Electrical Site Plan be submitted, and the Owner/Applicant agrees and understands that all site lighting shall be arranged to deflect light away from adjoining properties and adjoining streets, and which will require full cut-off fixtures for exterior parking lot lighting and fully shielded fixtures for wall mounted exterior lighting. Shielded shall mean that 100% of the lumens emitted from the light fixture are projected below an imaginary horizontal plane passing through the highest point on the fixture from which light is emitted; all to the satisfaction of the Department Services Department.

Business Performance and Environmental Sustainability Department

- L. The Owner/Applicant will be required to retain a licenced experienced qualified person to demonstrate that the foundation supports will not puncture the Municipal Supply Aquifer to the satisfaction of the Business Performance and Environmental Sustainability Department in accordance with the City's Drinking Water Protection policy. A peer review of this study is a requirement of the City of Barrie at the full cost of the Owner/Applicant.
- M. The Owner/Applicant will be required to retain a licenced experienced qualified person to provide a Contaminant Management Plan to support the proposed construction works to the satisfaction of the Business Performance and Environmental Sustainability Department in accordance with the City's Drinking Water Protection policy. A peer review of this study is a requirement of the City of Barrie at the full cost of the Owner/Applicant.
- N. The Owner/Applicant will be required to retain a licenced experienced qualified person to demonstrate that permanent dewatering is not required to the satisfaction of the Business

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Performance and Environmental Sustainability Department and Development Services Department in accordance with the City's Drinking Water Protection policy. A peer review of this study is a requirement of the City of Barrie at the full cost of the Owner/Applicant.

- O. If temporary dewatering is required, the Owner/Applicant will be required to retain a licenced experienced qualified person to provide a Hydrogeological Study detailing a proposed temporary dewatering plan to demonstrate no negative impact to the Municipal Supply Aquifer to the satisfaction of the Business Performance and Environmental Sustainability Department. A discharge agreement is required to be secured for any required temporary dewatering prior to issuance of a building permit, including consideration of a conditional permit for footing and foundation.
- P. The Owner/Applicant will be required to retain a licenced experienced qualified person to provide a D-4 Assessment of the site in accordance with the Ministry of the Environment Guideline for Land Use on or Near Landfills to the satisfaction of the Business Performance and Environmental Sustainability Department. A peer review of this study is a requirement of the City of Barrie at the full cost of the Owner/Applicant.
- Q. The Owner/Applicant will be required to retain a licenced experienced qualified person to provide a Fill Management Plan for any fill received and/or removed from the subject site in accordance with the City of Barrie Site Alteration By-law 2014-100 to the satisfaction of the Business Performance and Environmental Sustainability Department.
- R. That the drawings be revised as necessary, to the satisfaction of the Business Performance and Environmental Sustainability Department, to include:
  - i) Snow storage and catch basin locations with reference to treatment of meltwater collected downstream from snow storage areas; and,
  - ii) A property line maintenance hole and/or sampling port for the purpose of collecting sanitary sewer samples in compliance with Sewer Use By-law 2021-002.

Lake Simcoe Region Conservation Authority (LSRCA)

- S. That prior to the execution of the Site Plan Agreement, the Owner shall pay all development fees to the LSRCA in accordance with the approved Fees Policy under the *Conservation Authorities Act*.
- T. That prior to the execution of the Site Plan Agreement, the following shall be undertaken to the satisfaction of the LSRCA, in accordance with the Lake Simcoe Protection Plan and the Lake Simcoe Phosphorus Offsetting Policy:
  - i) Phosphorus budget
  - ii) Offsetting or Compensatory measures if required
- U. That prior to the execution of the Site Plan Agreement, the following shall be undertaken to the satisfaction of the LSRCA in accordance with the Lake Simcoe Protection Plan Water Budget Offsetting Policy for LSPP 4.8-DP and 6.40-DP:
  - i) Water Balance
  - ii) Offsetting or Compensatory measures if required
- V. That prior to the execution of the Site Plan Agreement, an ecological offsetting strategy must be submitted and implemented to the satisfaction of the LSRCA. Should the Owner/Applicant choose compensatory measures over feature offsetting, the required compensation to address feature loss will be \$15,745.85.



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- W. That the Owner shall agree in the Site Plan Agreement to only clear on-site vegetation outside of the bird nesting season (Note – Nesting season is April 1 – August 31).

Alectra Utilities Corporation

- X. That the Owner/Applicant shall comply with all requirements of Alectra Utilities Corporation as related to electrical servicing for the development, as stated in their "Conditions of Service" document. In this regard, the Owner shall submit an application for hydro service, receive approval from and execute an Offer to Connect agreement with Alectra Utilities Corporation prior to the commencement of construction. In the case where a transformer is required, any revisions to the site plan that are required as a result of relocating an Alectra owned transformer and high voltage underground service, may be subject to a Site Plan Amendment application.

Bell Canada

- Y. The Owner/Applicant shall agree to grant Bell Canada, any easements that may be required, which may include a blanket easement, for communication/telecommunication infrastructure. In the event of any conflict with existing Bell Canada facilities or easements, the Owner/Applicant shall be responsible for the relocation of such facilities or easements.

Development Services – Parks Planning

- Z. The Owner/Applicant will be required to retain a qualified Landscape Architect (and Arborist as applicable) to provide the design, inspection, and certification of all landscape works, all to the satisfaction of Park Planning.
- AA. That the Owner/Applicant submit an Inventory/Assessment by a qualified consultant (or Arborist as applicable), of all existing vegetation and natural features on and adjacent to the site, with preservation recommendations and details to be approved and coordinated with the application for a Site Alteration Permit, and or Grading Plan submissions, all to the satisfaction of Parks Planning.
- BB. That the landscape plans be amended, as required, to reflect recommended revisions concerning landscape areas, treatments, planting densities, screening/fencing, outdoor amenity spaces, site furnishings and pedestrian linkages, in accordance with the City of Barrie Urban Design Manual, all to the satisfaction of Parks Planning.
- CC. That the Owner/Applicant provide a letter of clearance pertaining to the Endangered Species Act that demonstrates that the site is clear of any flora or fauna identified under the Act. The letter must be received prior to the commencement of any site works and as a condition of registration. In the event the site contains any endangered species, it is the responsibility of the Owner/Applicant to contact both the City of Barrie and the Ministry of Natural Resources and Forestry and to take appropriate action.

Financial

- DD. That the Owner/Applicant pay the required cash deposits, securities and administration fees associated with site plan development for the following:
- i) Letters of credit in the approved format and in accordance with Council Policy 07-G-016 for appropriate works (such as drainage, servicing, grading and landscaping) within the boundaries of the site plan, equal to 50% of the value of those works (to a maximum of \$500,000 and a minimum of \$10,000.00) to the satisfaction of the Development Services Department;

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- ii) Letters of credit in the approved format and in accordance with Council Policy 07-G-016 for municipal works (such as roads and servicing) outside of the site plan boundary, equal to 100% of the value of works to the satisfaction of the Development Services Department;
- iii) Proof of the Owner's general comprehensive liability insurance policy in the amount of \$5,000,000.00 naming the City of Barrie as an additional insured;
- iv) Administration fees for the review and inspection of site servicing and landscaping works equal to 5% of the estimated cost of site servicing (minimum \$1,000.00) and 5% for landscaping (minimum \$500.00) plus applicable taxes;
- v) Any water service charges arising out of, or attributable to the development of the site plan including tapping fee and water meter payment;
- vi) Cash deposit for road clean up associated with the site construction (\$2,000.00 refundable deposit made payable to the City of Barrie);
- vii) A retainer in the amount of \$4,000.00 payable to Legal Services Department, City of Barrie, for legal and administration fees associated with the preparation and registration of the site plan agreement (additional fees may be required);
- viii) Administration fees of \$1,875.00 associated with the City of Barrie Development Services Department.

Building Services

- EE. That the fire access route conforms to the Ontario Building Code and that the Owner/Applicant enters into a fire route agreement, if required.

Development Services - Planning

- FF. That the Owner/Applicant provide confirmation that the property(ies) can act as one for the purpose of zoning by-law conformity. Any legal real estate transaction required to achieve this consideration is the responsibility of the Owner/Applicant and confirmation of that action is to be provided to the City prior to registration of the Site Plan Agreement. If applicable, a Deeming By-law application is required to be submitted and a By-law approved.
- GG. That the private streets/driveways be named, and units be numbered to the satisfaction of the City if and as required.
- HH. That the Owner/Applicant be responsible for the preparation and posting of any and all private street/driveway signs in accordance with current City standards. In this regard, the Owner/Applicant shall ensure that private street signs will display the street name and include the word 'Private' below the street name for clarity and that all costs associated with the installation and maintenance of private street name signs shall be the responsibility of the Owner.
- II. That the Owner/Applicant agrees that the construction of the building(s) shall be in conformance with the approved, registered site plans as it relates to the building design, construction materials and quality.
- JJ. That all sign locations be identified on the plans and details be provided and be in compliance with the City of Barrie Sign By-law 2005-093.
- KK. That the Owner/Agent ensure that all plans are consistent throughout.

File: D11-020-2020, 51-75 Bradford Street & 20 Checkley Street

- LL. That the plans conform to all provisions of the City's Comprehensive Zoning By-law or approval by the Committee of Adjustment for any variances be granted.
- MM. That the Owner/Applicant agrees to the dedication of, or cash-in-lieu payment of parkland in accordance with the *Planning Act*, if applicable, in a manner satisfactory to the Finance Department and the Development Services Department.
- NN. That the Owner/Applicant be responsible for obtaining the necessary approvals from any other applicable agency, if and as may be required.
- OO. That the Owner/Applicant agrees and understands that all garbage and recycling containers are to be kept inside the building(s) in an appropriate garbage room or externally within an enclosure (fully enclosed with roof and roll up door) constructed of materials similar to that of the main building and screened from public view, otherwise the City shall act as the Owner's agent and will have the containers removed at the Owner's expense.
- PP. That all roof top mechanical devices be identified on the plans and shall be screened from public view by way of roof top location or by way of a parapet building extension to the satisfaction of the Development Services Department.
- QQ. That prior to the registration of the Site Plan Agreement, the Owner/Applicant shall provide written confirmation that all conditions of the Lake Simcoe Region Conservation Authority (LSRCA), the Development Services Department (Approvals, Parks Planning, Transportation Planning and Planning Sections), Business Performance and Environmental Sustainability Department, Fire Services, and the Building Services Department have been completed to their satisfaction.
- RR. All final plans must be identified on the Site Plan Application Release form with signatures by the respective Departments and Agencies to confirm acceptance of all final plans. Three (3) full size prints of the final plan package, stamped and signed by the associated professional consultant, and a digital copy (high quality PDF without security settings) are to be provided to Development Services (Planning) prior to registration of the Site Plan Agreement. The Final Plan package forms a Schedule in the Site Plan Agreement.
- SS. The Site Plan Control Requirements shall remain in effect for a period of two (2) years from the date referenced below following which a subsequent application may be required.
- TT. That if a building permit is not issued within three (3) years of this approval, this Preliminary Approval shall become null and void.
- UU. That the Owner provide a letter or provide signature below, agreeing to the above Site Plan Control Requirements prior to any building permits, conditional or otherwise, being considered.

**Approved by Council Direction XXXXXX dated XXXXXX, 2021**

**Owner acceptance and agreement with the SITE PLAN CONTROL REQUIREMENTS FOR PROCESSING**

Please sign, date and return to the City of Barrie Development Services Department.

\_\_\_\_\_  
Owner's Signature

\_\_\_\_\_  
Date





**DEVELOPMENT SERVICES**  
**Urban Design Review**

**TO:** CELESTE KITSEMETRY, SENIOR PLANNER  
**NOTED:** JORDAN LAMBIE, SENIOR URBAN DESIGN PLANNER  
**RE:** D11-020-2020: 50 BRADFORD STREET – 3<sup>RD</sup> SUBMISSION  
**DATE:** APRIL 16<sup>th</sup>, 2021 (Update May 31, 2021)

**Key Planning Issues**

**1. Lakeshore Streetscape Design & Access**

**a. Additional Information Required – Applicant to Provide Updated Plans/Elevations**

The current design proposal creates an automobile-dominated streetscape along Lakeshore Drive and the waterfront, with a vehicular drop off area separating the building from the streetscape. Staff recommend the building should more directly engage Lakeshore Drive and provide active and publicly accessible ground floor uses along Lakeshore Drive (such as the restaurant) to create a pedestrian-oriented development with a strong connection to the waterfront. Multiple uses and pedestrian entrances along the Lakeshore Drive frontage are encouraged.

**b. Applicable TAC Standards Have Been Met**

The current location of the vehicular access on Lakeshore Dr. is proposed at the south side of the site to meet TAC standards established by speed limits and stopping sight distances. A recent Council motion to explore speed limit reductions to 40km/h in the downtown may permit the consideration of locating the access to the north side of the site, potentially requiring a stopping sight distance of 85m instead of 105m.



*Demonstration of an 85m sight line from the signalized intersection of Toronto Street and the Marina Access to the approximate location of the proposed driveway on the subject site.*

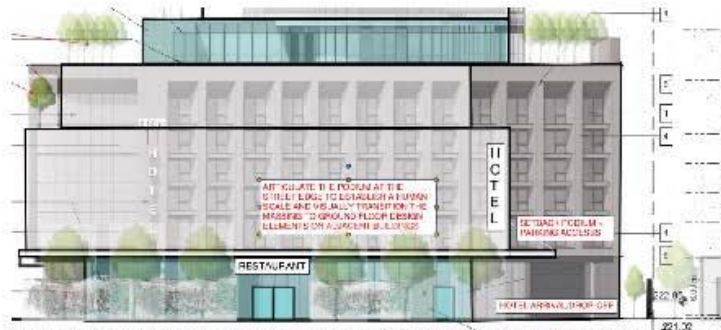
c. Applicant exploring alternative design strategies to address i and ii below in updated plans/elevations.

Given the impacts the proposed drop-off area will have on Lakeshore Drive and the waterfront, Urban Design staff recommend further exploring a reduced speed limit along Lakeshore Drive to facilitate the relocation of the site access to the north side of the site. This will serve to:

- i. create a pedestrian-friendly streetscape experience in the City Centre by consolidating, and mitigating the physical impacts of vehicular uses on the site and surrounding public realm (i.e. Lakeshore Drive streetscape, Urban Walkway, and EP area).
- ii. minimize the visual impact of parking areas and accesses by co-locating the proposed parking access and driveway immediately adjacent to the existing parking access of 2 Toronto Street.



Example of an Urban Hotel Development Providing an Engaged Street Edge with a Restaurant at grade and a Combined Hotel Drop Off Area/Parking Access.



A markup of the proposed east elevation demonstrating a similar compact drop off area on the subject site and refined articulation of the setbacks and step backs to improve massing design and transition to surroundings.

2. Additional Information Required – Applicant to Provide Updated Plans/Elevations  
Urban Walkway

- a. The proposed 'Urban Walkway' should provide both circulation and amenity space, and provide a well-designed, high quality urban space. The urban walkway should provide a clear, linear connection between Checkley and Lakeshore Drive/the waterfront to improve wayfinding and circulation. Staff recommend exploring an increase in the width of the urban walkway to 15m (currently 10m) to facilitate dedicated circulation, amenity and landscape zones within the space.



- b. Staff would like to continue working with the applicant to achieve an enhanced landscape design and high-quality pedestrian environment that:
  - iii. Considers canopies/awnings or other such articulations along the first floor to further define the human scale of the podium.
  - iv. Clearly defines spaces for different functions and activities along the urban walkway. (i.e. circulation zone, active or passive amenity areas).
  - v. Provides landscaping with a mix of hardscapes and softscapes; including additional furnishings, plantings, signage and wayfinding elements, lighting, public art etc.



The uniform condition of the proposed 'Urban Walkway' (left). Example of a circulation space functioning as a 'Linear Park' with clearly defined circulation and activity zones and featuring high quality landscape design, plantings, furnishings, lighting, signage, and wayfinding (right)



Markup of proposed plan, illustrating a vehicular site access at the northern edge of the site, a widened and linear urban walkway condition, and improved Lakeshore and Checkley Streetscape conditions with multiple street level uses to animate the public realm.



## PODIUM DESIGN

### 1. Massing, Articulation and Materials

#### a. Additional Information Required – Applicant to Provide Updated Plans/Elevations

Staff recommend breaking down repetition in the length of the façade by adding visual interest and variety while maintaining balance and symmetry in the elevation (through changes in design and/or materials). Façade design and articulation should help frame key building entrances and/or site features.

#### b. Additional Information Required – Applicant to Provide Updated Plans/Elevations

Staff generally do not support the proposed cantilevered drop-off condition; preferring a built form design more in keeping with the *Intensification Area Urban Design Guidelines* and which directly engages and activates Lakeshore Drive with active building frontage, direct pedestrian entrances from the sidewalk and other streetscape activity.

#### c. Additional Information Required – Applicant to Provide Updated Plans/Elevations

The massing, architectural style and material colours of the podium are generally inconsistent with the surrounding neighbourhood character/context. The contrasting architectural styles of an institutional-looking podium and modern tower, generally create a perception that these two elements are not part of a unified structure/building.

#### d. Meets Site-Specific Zoning By-Law

A 23.81m height is proposed for the 6 storey podium building. The *Intensification Area Urban Design Guidelines* indicate that buildings should not exceed a 45 degree angular plane taken from 80% of a R.O.W width. Staff understand the proposed width of Checkley Street will be approximately 18.6m. The podium should therefore step back in height after 14.8m (or approximately after the 4th storey). Staff generally do not support anything greater than a 1:1 ratio of building height to street width (from building face to building face). Staff recommend reducing the podium height or including step-backs on the upper levels of the podium.



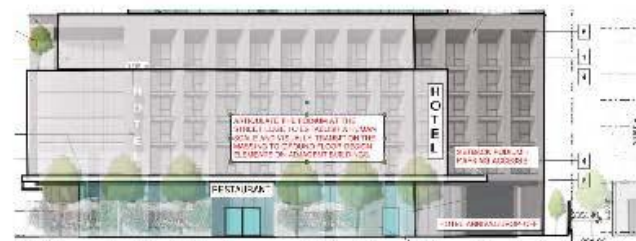
Diagram from the *Intensification Area Urban Design Guidelines for Creating Human Scale Streetscapes*.



*The proposed south elevation.*



*South elevation markup illustrating 1. step-backs on the podium to create a more human scale streetscape, 2. articulation to break up repetition in the façade and frame key entrances; and 3. an extension of the built form to engage and animate Lakeshore Drive with active ground level uses.*



*Sketch markup illustrating improved articulation and transition in the massing using variations in setbacks and step backs.*



*The proposed dark brick podium contrasts with surrounding neutral materials and colours.*



**DEVELOPMENT SERVICES**  
**Urban Design Review**

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*Example of a modern building with more compatible material colour palette.*

## **TOWER DESIGN**

### **1. Location and Massing**

#### **a. Meets Site-Specific Zoning By-Law**

While the proposed tower location may serve to facilitate views to the water for future phases of the proposed development, its location appears disconnected related to any site-specific vision or considerations to the surrounding context. Given the site's prominence on the City's waterfront, staff had requested the applicant provide an addendum to the Design Brief to include a design vision for the tower and a rationale for its location. Two simple concepts were provided as examples:



*Two example design concepts with distinct visions and associated rationales for tower location:*

1. *Tower located at corner of Lakeshore and EP lands, partially breaking through the podium massing to mark the gateway location of the EP area / Waterfront while also providing additional separation distance to the towers at 2 Toronto Street (left). Staff recognize that this example may impact planned views from future phases and deviate from the zoning concept approved in principle.*
2. *Tower location setback from EP lands to accommodate a green terrace building design that carries the green character of adjacent EP lands into the building design (right).*

### **2. Tower Articulation and Materials**

#### **a. Additional Information Required – Applicant to Provide Updated Plans/Elevations**

In general, the proposed architectural design and material selections of the building are not context sensitive, making the building feel 'out of place'. Tower design should be more sympathetic to, and compatible with, the existing character of the Lakeshore Drive corridor.

#### **b. Additional Information Required – Applicant to Provide Updated Plans/Elevations**

Given the size of the proposed floorplates and height of the tower, staff recommend:

- i. Reducing the visual impact of the building mass by adding further articulation in the tower design and variety in tower materials.
- ii. Reducing the length of balconies and adding variation in their design to break down the overall mass/width of the tower, add visual interest, and be more consistent/compatible with the existing buildings along Lakeshore Drive. Large balconies often become unsightly and used for outdoor storage.
- iii. Using the datum line of the existing height limit along Lakeshore Drive to recognize the surrounding context and provide changes in massing and materials. Massing, architectural design and materials:
  - o below the datum line should be compatible and complementary to the existing structures along Lakeshore Drive and the proposed podium.



- o above the datum line should become increasingly transparent to minimize the visual impacts of the proposed additional height above the datum.



Detail of proposed tower with unbroken balcony design, creating a repetitive and uniform slab appearance (left). Rendering of the proposed tower contrasted against the existing towers along Lakeshore Dr which use smaller and punctuated balcony designs and tower articulation with a variety of materials to break down the visual mass of the buildings. Datum line created by existing height limit indicated by dashed line. (right).



*Examples of articulation in tower design; punctuated balconies and material transitions provide variation and visual interest and help to mitigate the visual impact of the height and length of the tower's mass.*



DEVELOPMENT SERVICES DEPARTMENT

## MEMORANDUM

**TO:** Celeste Kitsemetry  
Senior Planner

**DATE:** May 31, 2021

**FROM:** Nadine Rush, C.E.T.  
Senior Development Services Technologist

**RE:** **Site Plan**  
**51 Bradford Street,**  
**Review Comments, 3<sup>rd</sup> Submission**

File: D11-020-2020

Further to your invitation to provide comments on the above noted site plan application, please be advised that the Development Services Department – Approvals Branch has completed a preliminary review of the application and recommends that the following special and standard conditions be applied.

Please be advised that the Development Services Department requires an opportunity to provide further comments on any revised submissions.

### Approvals Branch

1. Water servicing to be addressed as per comments provided by Water Operations.  
  
The following comments have been provided by Corporate Asset Management, Infrastructure and Growth Management.
2. The Comment Matrix refers to Figure 5.1 of the Stormwater Management and Servicing Addendum however it is not included in the PDF document of the Report. It is expected that the overall servicing plan will include ultimate servicing strategy (i.e., adequate looping with minimum two supply feeds from different watermains) for the Site.
3. The recent fire hydrant test results have confirmed similar boundary conditions that are assumed in the City's current Water Model, and it is agreed that the available fire flows within the existing watermain system (Bradford Street and Lakeshore Drive) can adequately support the proposed development, based on the calculated FUS fire flow requirements for phase 1, and assumed 283 L/s of fire flow requirements for future phases.
4. For the interim condition of Phase 1 development, it is understood that fire supply will be provided via the proposed fire service to the building and a municipal fire hydrant is proposed on the Lakeshore Drive watermain system (300mmø). The number of fire hydrants required to provide the desired flow needs to be determined by the qualified professional engineer considering the expected maximum flow from a single hydrant and the maximum spacing between hydrants and building.





DEVELOPMENT SERVICES DEPARTMENT

-2-

File: D11-020-2020

51 Bradford Street

May 31, 2021

**STANDARD CONDITIONS**

**Site Plans**

Refer to the Site Plan Application Manual for standard conditions and financial requirements.

Refer to Barrie Standards and Storm Drainage and Stormwater Management Policies and Design Guidelines for design standards and policies.

For further information/clarification regarding the above conditions, please contact the following staff:  
Nadine Rush – Development Services - Approvals nadine.rush@barrie.ca

Prepared by:

A handwritten signature in black ink that reads "Nadine Rush".

Nadine Rush, C.E.T.  
Senior Development Services Technologist

NR/

SURFACE WATER  
TREATMENT PLANT  
20 ROYAL PARKSIDE DR  
TEL. (705) 792 7920  
FAX (705) 792 7921



P.O. BOX 400  
BARRIE, ONTARIO  
L4M 4T5

THE CORPORATION OF THE CITY OF BARRIE  
Infrastructure Department  
Water Operations Branch  
*"Committed to Service Excellence"*

**51 Bradford St (D11-020-2020)-3<sup>rd</sup> subm revised**

Date: May 25/21

**Comments:**

-The site servicing plans (Interim & Ultimate) submitted by SCS Consulting need to be revised as follows:

**Interim Servicing Plan**

- The city does not allow combined domestic/fire services as shown. The proposed 200mm fire service off the Lakeshore w/m needs to show the service valve being located back at the 300x200 tee and a separate 150mm domestic service be taken off the 300mm private w/m.
- all service valves to be located at tee's and all services should be mechanically restrained from tee to building (typical) -add note to drawing.
- the proposed hydrant off the Lakeshore w/m to be removed and re-installed off the private w/m within the site (beyond the check valve chamber). This hydrant will be private, not municipal.
- the 300mm service valve on the inlet side of chamber to be relocated back to the 300mm tee and 300mm w/m mechanical restrained from tee to 20m beyond the 300mm valve on the outlet side of chamber (add note to drawing showing limits of mechanical restraint)
- the last 20m of 300mm private w/m in the Interim Phase should be mechanically restrained (add note to drawing)
- all joints on the connection to the 300mm HDPE Lakeshore Dr w/m to be mechanically restrained (add note to drawing) with joints to the existing HDPE w/m (north and south connection) being fused x mechanical joint adaptors and then all other joints at tee's and valves to be mechanical joints restrained (add note to drawing)
- the chamber c/w single check valve needs to be installed as per BSD-525. -add note to drawing.
- All domestic water services require a same sized water meter (ie: 150mm domestic requires a 150mm water meter) c/w bypass assembly as per BSD-533 (typical) -add note to drawing.
- all water service risers (domestic & fire) entering buildings to be constructed as per BSD-505 (typical) -add note to drawing.
- All hydrants to be installed as per BSD-507 and private hydrants to be painted red (typical) -add note to drawing.
- All domestic and fire service risers entering building(s) to be constructed as per BSD-505 (typical) -add note to drawing.

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-Both domestic and fire services will require backflow prevention installed as per City of Barrie bylaw 2017-121 (typical) -add note to drawing.

-All existing water services that front development property will need to be abandoned by cutting off at w/m connection (typical) -add note to drawing.

**Ultimate Servicing Plan**

-all joints on the Bradford St w/m connection to be mechanically restrained (add note to drawing)

-the 300mm service valve located on the inlet side of chamber to be relocated back to the 300mm Bradford St tee.

-all joints on the 300mm private w/m to be mechanically restrained from 300mm tee connection to 20m beyond valve on the outlet side of chamber (add note to drawing)

-need to show all existing water services fronting property off the Bradford St w/m and a note indicating all services needing to be abandoned by cutting off at watermain connection.

-all proposed 150mm domestic services to be connecting to the 300mm private w/m and not tee'd off fire service.

Allan Miller,  
Water Operations Technical Advisor



**Transportation Planning  
Site Plan Application**

**To:** C. Kitsemetry, RPP, Senior Planner

**From:** J. MacDonald C.E.T., Senior Transportation Operations Technologist (Ext. 5178)

**Date:** May 26, 2021

**Re:** D11-020-2020 – 51 Bradford Street

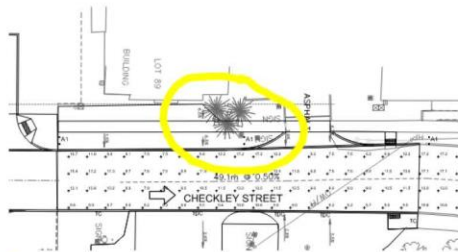
**Development Stage:** Site Plan Application

Introductory Statement:

Staff reviewed the proposed site plan (D11-020-2020) for 51 Bradford Street submitted to the City as part of the site plan application process.

Checkley Street Extension:

1. **Addressed**  
The existing three (3) trees within the Checkley Street ROW shall be removed.



2. **Pending Internal Review**  
Staff have reviewed the provided streetlight design and photometric plan for Checkley Street and have the following comments:
  - a. Calculations can be removed on Simcoe Street, only show Checkley Street calculation points.
  - b. The luminaire schedule has both PRO301 (Post-Top) and PRO351 (Pendant Style) Luminaires, please clarify which of the two are being proposed. Also BSD 826 & 827 are for the PRO301 (Post-Top) and BSD 828 & 829 are for the PRO351 (Pendant Style), please revise accordingly and re-submit.

Luminaire Schedule							
Symbol	Label	Manufacturer	Catalog Number	Description	Lamp	Number Lamps	Filename
•	A1	QUATTRO	PRO301-L3/HS-GPC-109LEDV4.0	PRO351 post top, 36 LED's, 4000K CCT, TYPE L3 OPTIC, CLEAR GLOBE with House-side Shield	(3) LED engine ARRAY(S) DRIVEN AT 1050mA	1	PRO301-L3HS-GPC-109LEDV4.0.IE S

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Property Conveyance / Corridor Protection:

3. **Addressed**  
The applicant acknowledges that Part 1 as identified on the Draft M Plan prepared by Scaeffler Dzaldov Bennett Ltd. Job No:19-047-04 dated August 10, 2020 shall be conveyed to the City of Barrie in accordance with Schedule E – Roadway Widening Plan of the Official Plan.
4. **Addressed**  
The Transportation Master Plan has identified an ultimate 34.0 m road allowance. In this regard, the owner shall protect for a future 1.5 m widening along the entire Bradford Street frontage by not locating any buildings or structures in this area. This shall be measured from future property line.

Exterior Lighting and Photometric Plan

5. **Addressed**  
All proposed exterior light fixtures are full cut off.
6. **Additional Information Required – Applicant to provide detail of screening**  
Staff have concerns regarding the proposed lighting levels along the northern portion of the site adjacent to 2 and 6 Toronto Street; to this regard the applicant shall provide screening or mitigation measures.

Items still under review:

7. **Change Required – Operational staff advised the proposed turn around is not acceptable and shall be updated to reflect a cul-de-sac. The cul-de-sac shall adhere to BSD-329.**  
Proposed turnaround to accommodate winter maintenance.
8. **Pending Internal Review**  
Staff are discussing internally the feasibility of the proposed pedestrian lighting within the environmentally protected lands as concerns have been raised regarding lighting naturalized environmentally protected land.



---

Justin MacDonald, C.E.T.  
Senior Transportation Operations Technologist





DEVELOPMENT SERVICES  
Parks Planning

MEMORANDUM

**TO:** Celeste Kitsetmetry  
Senior Planner

**FROM:** William McGregor  
Landscape Architectural Planner

**DATE:** May 12, 2021

**SUBJECT:** **51 Bradford Street**  
**Third Submission Comments – Parks Planning**

File: D11-020-2020

Celeste,

After a review of the 3rd submission, I have noted a significant number of items that may have been acknowledged but have not been fully or properly addressed. The outstanding/unsatisfactory items are noted in red and the acceptable items in green.

**Part A - Environmentally Protected Lands (EP)**

1. There needs to be a clear definition as to the extent and limits of EP boundaries in terms of what is being dedicated to the City and what will remain as part of the site plan.
  - Not provided in the landscape drawing sets, reference in comments to Grading Plan and a formal survey taking place after construction.
  - These limits need to be identified up front as part of the landscape submission.
  - The built form of the stairs and landing is not contained 100% within the site plan limits as per the grading plan provided by the Civil.
- a. It is understood by staff that the new EP limits will be defined by the revised flood limit shown on the February 23, 2021 Trail Impact Study.
  - These limits need to be identified up front as part of the landscape submission.
- b. There appears to be some confusion over the extent of dedication that needs to be clarified and reflected clearly on all Landscape submissions.
  - The built form of the stairs and landing is not contained 100% within the site plan limits as per the grading plan provided by the Civil.
2. There are currently two established 'trail head' locations for an east/west connection off Lakeshore Drive on each side of Bunkers Creek. The northern connection will need to be designed to connect through to Bradford Street as part of the submission requirements.
  - There is no site-specific design or layout provided in the landscape submission. A trail head should have signage, seating and garbage.
  - The transitional grading and approach/connection to the existing sidewalk should be shown for both trail ends.
- a. The Bradford Street connection is being shown as jogging back onto the site plan block for approximately 32 ln/m. The public walkway can not be located on the private site plan block for liability issues. The trail head design must be incorporated within the EP block.



DEVELOPMENT SERVICES – PARKS APPROVALS

File: D11-020-2020

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51 Bradford Street

May 12, 2021

- Walkway has been provided on the EP block.
- Trail head design has not been provided.
- b. The western portion of the trail should be offset from the site plan block taking into consideration the need for buffer planting, setback requirements as well as fencing in accordance with the City's fencing policy.
  - Trail has been offset but the buffer planting does not adhere to CEPTD principals. Direction provided to the LA consultant requiring a 1.0-1.5m mow/maintenance strip on each side of the walkway which is generally a sodded strip has not been provided.
  - 1.52m chain link EP fencing has not been shown on the plans.
- c. As part of the works within the EP block, it is expected that the existing trail will be upgraded to the meet the current City standard of 3.0m in association with the (2) two new crossings required to be installed as result of the incorporation of the SWMF drainage channels. These two crossings will need to be rated and sized for Parks Maintenance vehicles.
  - The walkway width with a 5.0m base is correct.
  - Specific direction has been provided on the two crossings, they can potentially use culverts which may be problematic with existing grades or a bridge structure is required to traverse the overland flow. **The use of Turf Stone is not acceptable.**
  - Staff had requested the reduction to a single overland flow channel to minimize the crossing requirements. This has not been addressed.
  - There is no seeding plan or soil specifications provided for the Turf Mat channels.
- d. The Lakeshore trail head will need to be enhanced and redesigned to accommodate the additional trail width expectation.
  - Only the width of the trail was addressed there are no other details provided.
  - Transition zone, benches, garbage, signage and layout have not been provided as part of the landscape submission.
- e. Please note that the updated redesign of this trail connection as well as the potential lateral connections if developed from the site plan including but not limited to the following:
  - i. Coniferous trees must be located a minimum of 3.0m from the edge of trail.
  - ii. Deciduous trees must be located a minimum of 1.5-2.0m from the edge of trail including shrubs.
  - Has not been adhered to see north west portion of trail
  - iii. A 1.5-2.0m maintenance strip is required adjacent to the trail. This makes the overall trail width requirements 6.0-7.0m in width. This was not reflected in the trails impact study by Beacon Environmental.
  - Not shown

DEVELOPMENT SERVICES – PARKS APPROVALS

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51 Bradford Street

May 12, 2021

- iv. Bench locations on concrete pads are to be provided.
  - Benches have not been included on the plans provided
- v. As per the standard for new trail development in and around a SWMF, the pedestrian link/trail will need to be 3.0m in width with asphalt. In order to accommodate the 5.0m maintenance road for pond clean out, the road base is 5.0m in width.
  - There is not direct access shown for the pond clean out, this should be included in the plans.
  - While the "bump out" may accommodate vehicles it has nothing to do with the actual pond access.
  - A BSD is referenced for pond access but there is no number or drawing reference.
3. The developer will be responsible for providing a Trail Study and detailed design for both the LSRCA and City to complete and upgrade the existing trail to the City standard.
  - a. Upon further review of the original comments there is an error in the staff memo in which the word "no" was omitted.

*"As part of this overall design, lateral connections from the site plan are welcome with the understanding that there are to be no additional trails or pathways twinning the east/west alignment as shown on the plans within the EP dedication."*

Although clarified in the last review meeting, the plans provided still show the twinned trails.
  - b. The increased infrastructure including the (2) additional bridges and secondary trail are not supported by staff within the EP designated lands.
    - To be clear, the reference on the (2) two additional bridges was discussed at length in the meeting with the Landscape Architect and was not intended to reference the trail which remains. These bridges should not have been removed. See also comment 2c)
    - The 2nd trail system was not supported by staff which would have required a total of (4) four bridge crossings.
  - c. This document will also speak to the planting requirements within this zone which are to be a "naturalized" planting with 50% coverage and will not contain maintained gardens. See also comment 2.d.
    - The planting within the trail impact study was not supported by staff. The Landscape Architect was responsible for providing the landscape planting details, layout and restoration associated with this project. In keeping with the requirements of the City.
    - The planting requirements have not been met and all that was provided was a colored overlay of the planting from the Trails Study.
    - Density charts are required to be provided in accordance with the planting standards showing the amount of deciduous trees, coniferous trees and shrubs based on the overall area of disturbance. This reference material had been provided to the Landscape Architectural consultant during the video meeting.
    - To be clear the percentage coverage requirements for the naturalization area are based on 50% coverage. This coverage does not include the required

DEVELOPMENT SERVICES – PARKS APPROVALS

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51 Bradford Street

May 12, 2021

- planting density within the site plan which can be found in the urban design manual.
  - All plantings must be native and zoned appropriately for Barrie. The planting list contains numerous unsuitable plant species.
  - All upland seeding must be the Simcoe County Native Seed Mix with an annual rye nurse crop.
  - The original formal garden plantings have not been removed, just the walkways.
4. It is the understanding of Parks Planning that the ECO Pond has been deemed suitable as an outlet location for the two outflow channels from the site plan. The previous comment from this department regarding its use is retracted. Please note however, that maintenance access will have to be accommodated for future clean out or other operations which should be in keeping with the upgraded access requirements.
- Parks Planning does not provide authorization for requirements relating to access or cleanout requirements for the pond and its maintenance function. We may have discussed using the trail as shared access but that is the extent of our approval.
5. The overland flow routes will not be permitted to cross the trail network at grade and culverts will not be entertained as a crossing option. If it is determined that the overland flow routes must cross the trail network, it shall be the developer's responsibility to provide adequate crossing structures to support service vehicles and pedestrians with a minimum width of 3.0m at each location.
- Turf stone is not an acceptable surfacing for crossing, these are to be bridge structures.
- a. Staff have looked at the current plans and there are (4) four noted crossing structures. Staff will only support the two structures shown on the existing trail.
- Structures were removed.
- b. Is it possible to link the two outlets into a single outfall structure to reduce the crossing needs?
- Acknowledged
- c. The preference from staff is that the outlet swales should receive a "green" treatment. The use of cable mats, turf stone or limestone will not be supported by Parks Planning which will also provide an aesthetic bonus for the view from the hotel and tower(s).
- Seeding specifications not detailed on the landscape plan.
  - Minimum topsoil depth is generally a min of 200mm and now 100 as specified.
  - Topsoil depth for upland planting is to be 400mm-1000mm in depth for the EP lands in areas of disturbance.
6. The conceptual layout shows several retaining walls, stairs, and access ramps. These structures must be constructed solely within the limit of the site plan and will not be maintained by the City. It should also be noted that there will be no winter maintenance on any of the proposed trails within the EP dedication (as to be determined).
- Based on the development limits shown and referenced in the comment matrix, the stairs and concrete landing are not contained within the limit of the site plan and do extend beyond the EP boundary.



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- a. MHBC has provided a detail from the retaining wall profile (*being No. 16 on L-2.2 and No. 17 on L-2.0*). Please note that these details refer to a side slope of 3:1 (30%). As of 2019, any site in which maintenance is or may be required by Parks Operations, a maximum of 4:1 is to be applied. If there will be no maintenance requirement on this slope 3:1 is applicable for a "naturalized" slope.
  - b. A detailed review of grading has not taken place at this time.
  - c. Structures such as stairs or ramps are to meet and match existing grade at the development limit only.
    - Not represented on the plan, a formal survey following construction has no bearing on this matter as noted in the comment matrix. The boundaries are clearly shown and the structure extends past that limit. The stairs do not define the EP boundary as a matter of convenience after construction as lands must be dedicated to the City.
7. In keeping with current restoration and naturalization practices, the disturbed areas and buffer zones of the EP dedication are to be naturalized in accordance with City guidelines at either 50% or 35% coverage in accordance with direction from the LSRCA. The City will review and approve plans in accordance with our standing agreement with the LSRCA.
- Planting layouts, density and material selection do not meet the requirements of the City of Barrie or the governing documents. This work remains incomplete.
  - a. The restoration drawings are to be part of the Landscape submission by the Landscape Architectural consultant and be in keeping with the City's current planting guidelines contained within the Storm Water Policy Manual.
    - Density chart has not been provided and plant selection and placement do not follow the requirements generally. This will require additional conversations with the Landscape Architect to resolve.
  - b. Parks Planning does not endorse the restoration plan provided by Beacon Environmental shown as a "Conceptual Restoration Planting Plan" as it is not in keeping with the current planting policies. Beacon is welcome to contribute to the project for MHBC's landscape submission.
    - See previous comments, does not comply.
    - Additional clarification needs to be resolved in terms of scope of works. Within the EP there are two different applications:
      - The restoration/naturalization of disturbed and reclaimed areas. This standard for planting is based on 50% coverage.
      - The second unaddressed issue is the stream restoration works that are to be undertaken by Smart Centers on the City's behalf. The planting and naturalization in this zone for density will be determined by the LSRCA and the works are not directly associated with the naturalization works discussed to date. This is a much more complicated design process that has not yet been vetted with this department from a planting perspective. This work also does not contribute to the density calculations for the afore mentioned naturalization and trail works.



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8. Following discussions with the City of Barrie's Engineering Department regarding the works proposed for Bunker's Creek, it is currently the position of staff that as a sidewalk network is provided on Lakeshore Drive and Bradford Street that a mid-block crossing of Bunkers Creek is not required and that it may be problematic to facilitate the crossing based on the current channel design, flood restrictions and geological conditions.
  - a. The mid-block crossing is still being shown on the plan(s) provided. This structure and routing are not supported by staff.
    - Acknowledged
9. Further redline revisions are not being provided for the MHBC 2<sup>nd</sup> submission. Staff are more that happy to meet with the consultants to further discuss the City's expectations for the development of the Environmentally Protected lands that address our original comments, practices and standards moving forward and suggest that a design charette may be warranted given the scope of proposed works at the applicant's discretion.
  - There are still significant outstanding items from the March 16<sup>th</sup> 2021 meeting that are outstanding on the plans.
10. Moving forward with the EP restoration planting and trail the following needs to be provided:
  - a. Density charts showing the required planting based on area of disturbance and what is being provided.
  - b. Planting layout and groupings as per the guidelines.
  - c. Native plant material only.
  - d. Coniferous and deciduous tree densities appear to be quite low, this will be verified with the density charts.
  - e. Seeding for all upland or disturbed areas are to be Simcoe County Native Seed Mix applied at a minimum rate of 25kg per hectare. Wet meadow mix is only used around the waters edge of SWM ponds.
  - f. The pathway is to be asphalt, has this been brought to the LSRCA's attention and are they supportive or do we have to go to a granular option?
  - g. The trail is being identified as a Multi-modal route and the suggestion of lighting for the pathway has been made by other groups. Is the ground water and substrate material suitable to support light standards and conduit. Any lighting within the scope of the EP must be included on the Landscape drawings.
  - h. The electrical plan for the site plan includes bollard lighting within the EP. This is to be removed and the base information in the plan is out of date.
  - i. Benches, site furnishings and signage are to be included in the landscape plans.
  - j. The EP fencing adjacent to the site plan is to be included in the Landscape plans. The referenced detail for a shorted fence (location unknown) is incorrect in the set.
  - k. Planting sizes/caliper are to be as per City requirements, plant material in the plant list is undersized.
  - l. Herbaceous plant material in the planting list is generally not required for upland style plantings.
  - m. Vehicular controls will be required along both the Bradford St. and Lakeshore Drive frontage including vehicular control gates.
  - n. Any access for clean out to the pond should be defined by wooden bollards.

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May 12, 2021

**Part B - Hotel Development Site**

1. As an arterial roadway and as part of the premier waterfront, there should be a 6.0m transitional vegetative buffer at Lakeshore Drive.
  - While the comment matrix identifies an acknowledgement that the densities are difficult to meet there has been no visible attempt to bolster the planting opportunities that do exist.
  - A density chart for meeting the requirements of the guidelines needs to be provided as part of the landscape submission. Reasonable attempts to meet these requirements need to be made. This could also include roof gardens, planters, or other opportunities within the site plan.

Arterial/ Parkway/Major Collector	6m	<ul style="list-style-type: none"> <li>• Entry feature/ornamental fencing</li> <li>• Mixed Deciduous and Evergreen trees</li> <li>• Berming and shrub beds</li> <li>• Foundation planting</li> </ul>
Collector/Local	3m	<ul style="list-style-type: none"> <li>• Entry feature/ornamental fencing</li> <li>• Mixed Deciduous and Evergreen trees</li> <li>• Shrub beds</li> <li>• Foundation planting</li> </ul>

- a. This buffer does not include planting within the Municipal Right of Way.
    - Acknowledged
  - b. Any type of pedestal or roadway site identification would also require a base landscape treatment.
    - The pedestal sign base is not incorporated directly into a landscape bed. Confirmed by the notation of "sod" at its base.
  - c. There is a landscape treatment in the form of shrub plantings to the north of the proposed entry, these are shown to be partially on the municipal right of way in accordance with the property lines as shown.
    - They have been pulled back of the right of way but do not include the signage.
  - d. There are existing boulevard trees along the Lakeshore frontage. What trees are being impacted or removed to accommodate the proposed site works. For removed trees, their assessed value would be paid to the City and be put towards the municipal street tree planting program.
    - Acknowledged
2. The northern boundary is to include a 3.0m landscape strip along its length adjacent to the existing residential development.

Residential	3m	<ul style="list-style-type: none"> <li>• Mixed Deciduous and Evergreen trees</li> <li>• Mixed shrub bed or hedging</li> </ul>
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- Landscape buffer is shown and noted on the plan but it is not planted sufficiently. Refer also to comments pertaining to density requirements within the site plan.

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3. Based upon the original conceptual plan there was a negligible amount of available snow storage associated with the eastern entrance off Lakeshore Drive. The new plan shows a patio stone snow storage area directly off the laneway and adjacent to the existing development. Not only is this aesthetically questionable at the front of a hotel lobby but there is a missed opportunity for an enhanced landscape treatment. Snow storage for this site will most likely have to be hauled off site and sod is also an appropriate storage area. It should also be noted that the depositing of snow from winter maintenance operations within the EP will not be permitted.
  - Patio stones are still being referenced in the landscape plan although the comment matrix identifies these areas as sod. See L1, L1.1 and L1.2
4. As discussed in the previous section for Environmentally Protected Lands, it is unclear what the proposed limits of development are in relation to the scope of landscape works shown. These comments have been addressed in Part A but will have an impact on internal circulation and access expectations to the trail.
  - Has been previously identified and clarified in comments for the EP lands
5. The laneway will be private and not under Municipal control. It is noted that there are numerous planting beds shown adjacent to the laneway that include both boulevard trees and shrub planting. Please note that this material may be damaged during winter control operations for both the road and sidewalk which will serve as a drop off. Raised planters should be considered for annual or perennial grass displays and trees may be moved to the opposite side of the paved area. Irrigation or silva cells should be considered for this area.
  - Layout has not visible changed although the matrix indicates that this has been changed.
6. The planting area shown off the residential lobby in keeping with the previous comments may impede site lines coming out of the building parking structure.
  - Comment was addressed but this is an internal issue and any liability resulting from future maintenance or plant growth is that of the owner.
7. The Urban Design Guidelines for site plan development have specific density requirements. A summary of required and provided quantities should be provided as part of the drawing submission.
  - Density charts have not been provided.
  - There needs to be a clear distinction between Site Plan requirements and EP restoration requirements.
  - The listing provided does not discern between Creek, EP or Site Plan works as previously noted. Each area having a specific density requirement.
8. In accordance with City of Barrie Standards, an EP fence will be required along the entire southern limit of the property in keeping with the City of Barrie's council



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51 Bradford Street

May 12, 2021

approved fencing policy. Allowances may be considered for pedestrian connections through the design process. Refer also to comments in Part A.

- Wrong detail and Landscape Plan does not clearly show the fencing in the plan or legend.

9. It is noted from discussions that Checkley Street is to undergo upgrades associated with the existing road network. These road improvements should include municipal street trees where applicable.

- Checkley Street is not fully shown on the landscape plans.
- There are known trees that will have to be removed on Checkley Street to accommodate the road improvements/sidewalks.



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**Part C - Temporary Parking Lot (*South East of Site at Bradford Street*)**

1. As with previous comments, EP limits must be clearly shown.
2. In accordance with Part A, the trail is to be developed within the Environmentally Protected lands and is not to be routed onto the private site plan development.
  - Acknowledged
3. Although this parking lot is “temporary” there still needs to be accommodation of pedestrian circulation outside of the travelled laneways and this should also include some sort of interim landscape treatment. It is noted that a single external pedestrian path is shown but this does not address the south side of the parking or internal circulation. Parks Planning will defer to traffic on the circulation within the lot.
  - Circulation is addressed.
  - No interim planting provided.
4. As this is a very large asphalt surface, clear delineation of snow storage should be provided with the understanding that any runoff (melt) is not to be directed to the EP lands directly due
  - Acknowledged

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William McGregor  
Landscape Architectural Planner  
Development Services – Parks Planning

cc: Jordan Lambie, Senior Urban Design Planner  
Nadine Rush, Senior Development Services Technologist  
Kevin Bradley, Manager of Parks Planning

/WMc

BUSINESS PERFORMANCE AND  
ENVIRONMENTAL SUSTAINABILITY DEPARTMENT



## MEMORANDUM

**TO:** Celeste Kitsemetry, Senior Planner **File: D11-020-2020**

**FROM:** Katie Thompson, Risk Management Official  
Stephanie Zoschke, Supervisor of Environmental Compliance (Acting)

**DATE:** May 31, 2021

**SUBJECT:** D11-020-2020 – 3<sup>rd</sup> Submission  
51-83 Bradford St & 20 Checkley St  
Site Plan-Phase 1

The following items are outstanding and are required to be addressed to the Cities satisfaction.

1. A foundation plan completed by a Qualified Person demonstrating that the building supports will not cause contamination to migrate downwards to the municipal supply aquifer. **Foundations are not permitted to be founded in the Municipal Supply Aquifer.**
2. A contaminant management plan prepared by a Qualified Person is required to satisfy the City that measures are in place to mitigate impacts to the Municipal Supply Aquifer and Bunkers Creek. The contaminant management plan shall contain at a minimum the following:
  - i. Expected types of contaminant(s)
  - ii. Plan to monitor, mitigate, collect, treat, and/or discharge contaminated water
  - iii. If discharged, a contingency plan should the discharge water not comply with the limits for the receiving location
  - iv. Plan to ensure contaminated water does not migrate downwards impacting the municipal aquifer
  - v. The monitoring, maintenance, and mitigation requirements of the proposed French drain to be installed within the interim parking lot shall be included within the contaminant management plan, demonstrating there will be no impact to the creek.
3. Catch basins or trench drains capturing meltwater runoff for catchment 203 are not indicated on site plan drawings. It is noted that an OGS unit has been provided prior to channel 2. An

BUSINESS PERFORMANCE AND  
ENVIRONMENTAL SUSTAINABILITY DEPARTMENT

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May 31, 2021

updated drawing is to be provided indicating what method will be used for this catchment to route meltwater to OGS unit.

4. The Phase Two Environmental Site Assessment by GHD (Phase Two) was compared to 2011 MECP Table 1: Full Depth Background Site Condition Standards, Residential/Parkland/Institutional/Industrial/Commercial Property Use Standards as the presence of environmentally sensitive features are present on the Site. **The Phase Two is to be updated to reflect MECP Table 2: Full Depth Generic Site Condition Standards in a Potable Ground Water Condition.**
5. A property line maintenance hole and/or sampling port is required on the property for the purpose of collecting isolated discreet sanitary sewer samples from any industrial, commercial, or institutional premises in compliance with Sewer Use By-law 2021-002. The sample location should exclude any residential wastewater discharge. **The location of property line maintenance hole and/or sampling port shall be demonstrated on the Site Plan Drawings.**
4. The Phase 2 ESA identified the presence of imported fill material and potential for historic private waste to be located on the property. The City requires a **Ministry of Environment Guideline D-4 (Land Use On or Near Landfills) Study** be submitted to the City for Peer Review.
  - a. As per MECP Guidance Document “**D-4 Land Use On or Near Landfills and Dumps**” the D-4 Assessment is required to be Peer Reviewed by a third party reviewer for completeness and accuracy at the expense of the Proponent. The cost to peer review the D4 Assessment can range from approximately \$1,500.00 to \$5,000.00 depending on the complexity of the study.

Regards,



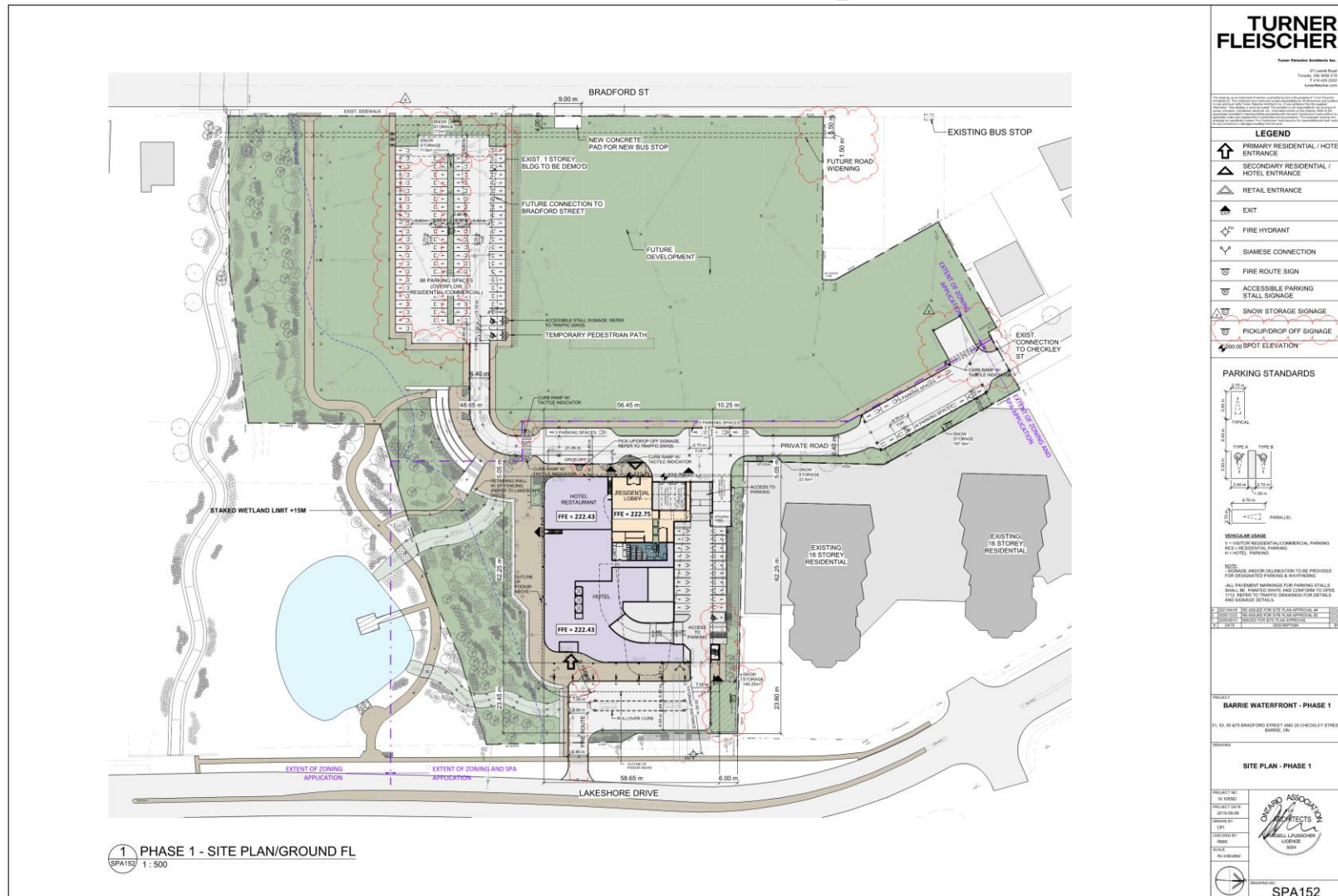
Katie Thompson, P. Geo  
Risk Management Official



Stephanie Zoschke, B.Sc, C.Tech  
Supervisor of Environmental  
Compliance, Acting







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# TURNER FLEISCHER

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PROJECT NO. \_\_\_\_\_

DATE ISSUED \_\_\_\_\_

PROJECT DATE \_\_\_\_\_

DATE REV. \_\_\_\_\_

DESIGN BY \_\_\_\_\_

DESIGNED BY \_\_\_\_\_

DRAWN BY \_\_\_\_\_

SCALE \_\_\_\_\_

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**BARRIE WATERFRONT - PHASE 1**

31, 33, 35 & 37 BRADFORD STREET & 33 CHECKLEY STREET  
BARRIE, ON

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**3D PERSPECTIVES**

PROJECT NO. \_\_\_\_\_

DATE ISSUED \_\_\_\_\_

PROJECT DATE \_\_\_\_\_

DATE REV. \_\_\_\_\_

DESIGN BY \_\_\_\_\_

DESIGNED BY \_\_\_\_\_

DRAWN BY \_\_\_\_\_

SCALE \_\_\_\_\_



ONTARIO ASSOCIATION  
OF ARCHITECTS  
1987

DRAWING NO. **SPA803**



APPENDIX "C"

Zoning Matrix



**Site Plan Application**  
*Zoning Comments*  
*M. Snow, Planner - Zoning*  
May 18, 2021

Site: 51 Bradford St

File: D11-020-2020

**Zoning:** Present: C1-2 (SP-602)

Proposed: C1-2 (SP-602)

**Standards**

	Required by By-law (C1-2)	Proposed
Lot Area (min.)	-	35,047m <sup>2</sup>
Lot Frontage (min.)	-	+50m
Front Yard Setback (min.) Lakeshore Drive	-	23m
Side Yard (min.)	-	+10m
Rear Yard (min.)	-	+10m
Lot Coverage (max.)	-	10%
Landscaped Open Space	-	66%
Building Height	26m within 10m of the front lot line, 88m beyond 10m	84.9m
GFA	30,000m <sup>2</sup> / 700%	29,115m <sup>2</sup> / 312%
Minimum Coverage for Commercial	15%	25%
Parking	1 space per dwelling unit (233 residential spaces for a total of 233 spaces required)	483 spaces provided (Barrier Free Parking to be provided in accordance with the AODA)
Loading Spaces	2 spaces	2 provided at the rear of the structure
Minimum Dwelling Unit Floor Area	35m <sup>2</sup>	Appears to be provided
Drive Aisle (min.)	6.4m	6.4m
Landscape Buffer	3m buffer required along the side and rear lot lines	Appears to be provided